

# Frodsham

## Town Design Statement



*Frodsham - Now & Then*

Adopted as a  
Supplementary Planning Document

July 2010

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1. Introduction to the Town Design Statement

1.1 Purpose and Objectives

The purpose of a Town Design Statement is to manage change in both landscape and buildings, whether large or small, in a way which reflects and harmonises with the local character of a settlement’s buildings, spaces and landscape setting.

The main aims of Frodsham Town Design Statement are to:

- augment the current Local Plan policies and influence the forthcoming Local Development Framework
- align with European, national, regional, county and local landscape policies
- promote a sense of place and acknowledge local distinctiveness
- encourage development which will make a positive contribution to Frodsham
- improve the acceptability of new developments by providing guidance to landowners, developers and architects before they prepare proposals
- encourage development which respects and complements existing building materials, but does not prejudice high quality contextually designed contemporary development
- encourage development that is designed to achieve high levels of sustainability and energy efficiency
- ensure that green infrastructure and biodiversity are an integral part of new development thus providing and/or maintaining habitats for wildlife as well as offering opportunities for people to get close to nature, with resulting benefits for health and wellbeing.

Positive benefits that may accrue from the Frodsham Town Design Statement include:

- raising community awareness of local design, distinctiveness and environmental issues
- creating partnerships between different groups in the community
- acting as a catalyst for new initiatives which may be started as a result
- streamlining the development process



The document will provide guidance to householders and local businesses, planners builders and architects and any other organisation or individual wishing to undertake development within the Frodsham Town Policy Boundary so that it contributes to and maintains the local character and distinctiveness of the town and its setting.

The Frodsham Town Design Statement, adopted as a Supplementary Planning Document by Cheshire West and Chester Council on 22nd July 2010, is a material consideration when assessing planning applications within the Frodsham Town Policy Boundary.

1.2 The Planning Policy Framework

The Frodsham Town Design Statement provides local definition to policies contained within the adopted Vale Royal Borough Local Plan First Review Alteration (Ref 1-2). In particular it supplements policies listed in **Table 1.1** below.

On 1st April 2009 Vale Royal Borough Council together with Chester City, Ellesmere Port and Neston Borough and Cheshire County Councils became a part of Cheshire West and Chester Borough Council. The new Council is currently working towards producing a Core Strategy, a document that will set out the core planning policies for the Cheshire West and Chester authority area as a part of its Local Development Framework (LDF). Under the Planning and Compulsory Purchase Act 2004 Local Development Frameworks and Regional Spatial Strategies have replaced the system of local plans and structure plans as the statutory development plan under which decisions on planning applications are taken.

The Vale Royal Borough Local Plan First Review Alteration was adopted on 16th June 2006. Under the 2004 Act the policies contained within the adopted Local Plan were originally saved for a period of three years from the date of the Local Plan’s adoption, to 16th June 2009. In March 2009 Vale Royal Borough Council’s request for this period to be extended was approved by the Government Office for the North West.

**The policies contained within the Vale Royal Borough Local Plan First Review Alteration will now remain in effect and continue to cover the former Vale Royal Borough Council authority area until such time as the Core Strategy for Cheshire West and Chester Borough Council is adopted.**

**When the Core Strategy is adopted, there will be a need to review this document and all other Village Design Statements adopted by the Council to ensure they reflect the policies it contains.**

Table 1.1 Local Plan policies applicable to Frodsham (Ref. Appendix 1)

Notation	Name of Policy
General Strategy	
GS3	North Cheshire Green Belt
Natural Environment	
NE1	Protection of the Nature Conservation Resource
NE2	Designated Sites of International and National Nature Conservation Importance
NE3	Designated Sites of Local and Regional Nature Conservation and Geological Importance
NE4	Threatened and Priority Habitats
NE7	Protection and Enhancement of Landscape Features
NE8	Provision and Enhancement of Landscape in New Development
NE9	Trees and Woodland
NE15	Protection of the Flood Plain
Built Environment	
BE1	Safeguarding and Improving the Quality of the Environment
BE4	Planning Obligations
BE5	Historic Environment – Listed Buildings
BE6	Alteration / Extensions to Listed Buildings
BE7	Changes of Use to Listed Buildings
BE8	Listed Buildings and Archaeology
BE9	Demolition Control of Listed Buildings
BE10	Historic Environment – Conservation Areas
BE11	Development Adjacent to Conservation Areas
BE15	Historic Parks and Gardens
BE16	Advertisements and Signs
BE19	Domestic Radio, Masts, Aerials and Satellite Dishes
BE21	Renewable Energy



BE22	Locally Important Buildings
BE23	Conservation Area Appraisals
<b>Housing</b>	
H4	Housing Development Hierarchy
H5	Windfall Sites
H6	Change of Use / Conversion
H7	Sub-Division of Existing Dwellings into Self-Contained Residential Units
H8	Extensions / Alterations to Dwellings
H10	Rebuilding / Replacement of Dwellings
H15	Sites for Affordable Housing within the settlement policy boundaries of Frodsham, Helsby and Tier 1 Locations
<b>Recreation and Tourism</b>	
NE4	Threatened and Priority Habitats
NE7	Protection and Enhancement of Landscape Features
NE8	Provision and Enhancement of Landscape in New Development
<b>Transportation</b>	
T5	Railway Stations
T8	Pedestrians and Walking
T9	Cycling
T10	National Cycle Network
T13	Car Parking
<b>Shopping and Town Centre Development</b>	
STC1	General Policy
STC4	Use of Upper Floors
STC5	Design, Signage and Advertising in New and Refurbished Premises
STC6	Installation of Shutters
STC8	Loss of A1 Uses
STC9	Loss of A2 and A3 Uses
STC10	Loss of Existing Ground Floor and Upper Floor Residential Uses
STC11	Large Proposals in Large Village Centres

1.3 Community Involvement in the Process

The Frodsham Town Design Statement has been produced by a working group of Frodsham residents under the banner of Frodsham Forward and facilitated by Cheshire Landscape Trust. The initial proposal to prepare the Frodsham Town Design Statement was made by John Gittins, Chief Executive, Cheshire Landscape Trust, in November 2003.

A series of meetings during 2004 resulted in a survey of the whole parish by local volunteers. This was completed in mid-2005. A draft document: Frodsham Parish Landscape Assessment and Town Design Statement SPD, prepared during 2005-06, was submitted for statutory and public consultation in August 2007.

Following a meeting with GONW in November 2007, the team focused on the area within the Town Policy Boundary in greater detail. The town was re-surveyed and the document was re-drafted and re-submitted for statutory and public consultation in June 2009. The diary of events is recorded in **Appendix 2**.

1.4 Monitoring and Review

Implementation of the Frodsham Town Design Statement SPD will be reviewed through the Annual Monitoring Report (AMR), a document which is intended to monitor all aspects of the Council’s LDF and is submitted to the Government Office for the North West each December.

The AMR provides information on whether the objectives of the SPD are being achieved and subsequently identifies if any parts of the SPD are in need of review.

The AMR monitors a set of National Core Output Indicators published by the Department for Communities and Local Government and Local Output Indicators. In reporting these indicators, the AMR takes an integrated approach which seeks to use these to monitor the objectives and policies contained in the Vale Royal Borough Local Plan First Review Alteration, including the policies on which this SPD is based.

The AMR will monitor how effectively the Frodsham Town Design Statement is being implemented through reviewing planning decisions within the Frodsham Town Policy Boundary and highlighting where the SPD has been used successfully as a material consideration. The SPD may need to be reviewed accordingly using this information.

Section 1 References

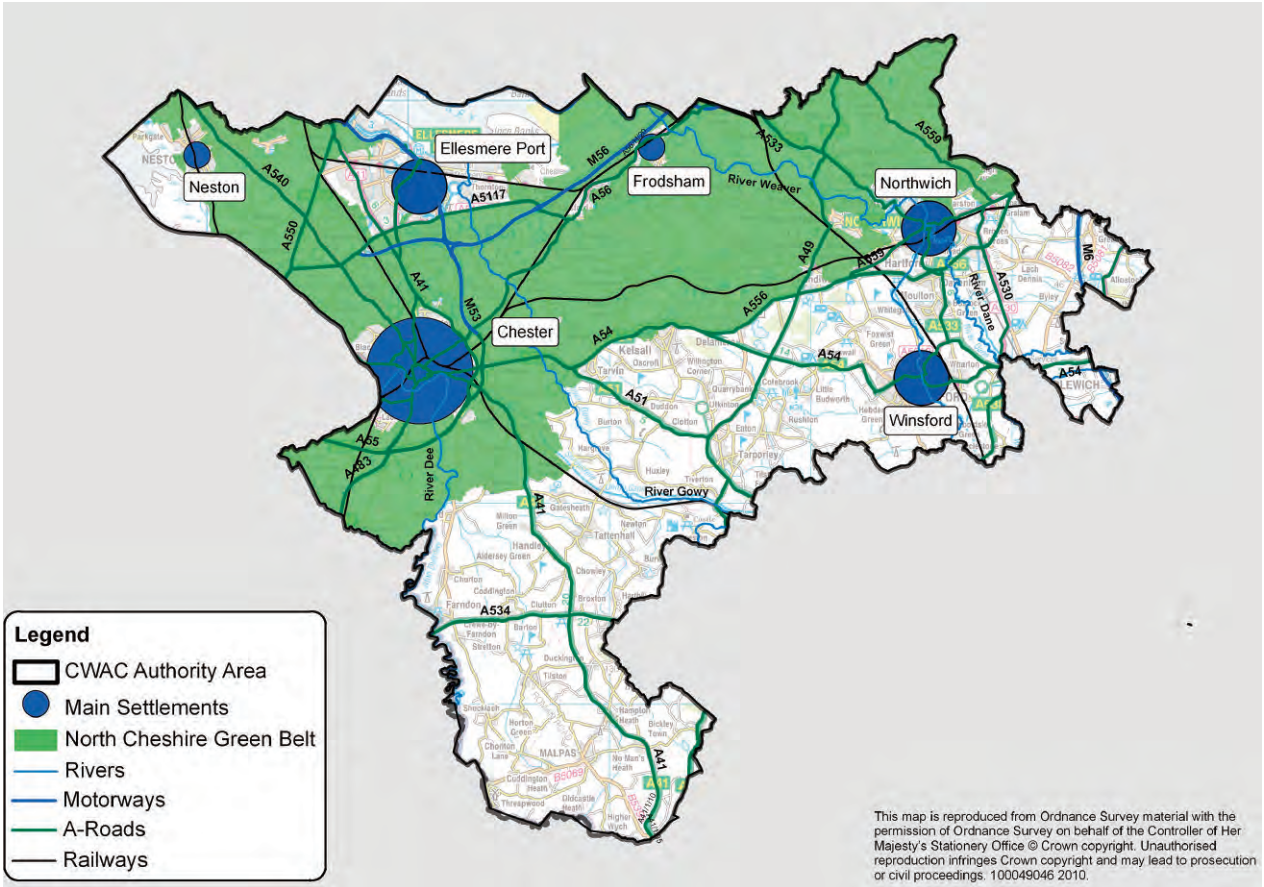
1. RBC (2006) Adopted Vale Royal Borough Local Plan First Review Alteration
2. VRBC (2006) Adopted Vale Royal Borough Local Plan First Review Alteration  
Proposals Maps
3. Cheshire West and Chester forthcoming Core Strategy
4. Council of Europe (2000) European Landscape Convention (in force from 2004)

2 The landscape setting

2.1 Location of Frodsham

Frodsham is a small town within the North Cheshire Green Belt and is located at the junction of three environments: the Mersey Estuary marshes, the mouth and valley-side of the River Weaver and the uplands of the Cheshire Sandstone Ridge. Since medieval times, and probably earlier, Frodsham has been accessible by river and road. The A56 Chester to Warrington road has long been the focus of economic activity within Frodsham and, more recently, a divider of the town as settlement boundaries expanded and the volume of traffic increased. Frodsham was connected to the railway network in 1850 and to the M56 motorway, skirting Frodsham to the northwest, in 1971 – **Map 2.1** shows Frodsham’s location within Cheshire West and Chester.

2.1 Location of Frodsham



2.2 Geology

The solid geology of Frodsham Parish, like most of Cheshire, mainly comprises sedimentary sandstones, siltstones and mudstones laid down in a hot desert environment during the Triassic Period. **Table 2.1** shows the sequence of rock units, their names and whether they visibly outcrop in the town.

**Table 2.1 Geological succession and visible presence in Frodsham**  
(Ref.1 & 2)

Period	Rock Group	Rock-Unit / Formation	Visible presence in Frodsham
Triassic	Mercia Mudstone	Sidmouth Mudstone Formation	No
		Tarporley Siltstones Formation	Yes
	Sherwood Sandstone	Frodsham Beds at the top of	Yes
		Helsby Sandstone	Yes
		Wilmslow Sandstone Formation	Yes
		Chester Pebble Beds Formation	No

Later earth movements tilted and lifted the rocks leaving evidence represented by a series of faults, most of them trending NNW to SSE and down-throwing to the east where the youngest rock is found. The Frodsham Fault can be seen in Howey Lane (photo 0.01); Wilmslow Sandstone is exposed in a stream course in Castle Park; Helsby Sandstone has been quarried for building stone in the south of the town; wind-blown dune structures can be seen in the exposure of Frodsham Beds, northeast of the railway station (photo 0.02); and laminated Tarporley Siltstones can be seen in Bridge Lane (photo 0.03).

As rocks were uplifted the land surface was worn away by rivers, then glaciers, as environments changed. On top of the Triassic rocks there is a patchy cover of drift deposits which accumulated during the last Ice Age. As ice retreated it left behind deposits of glacial till (boulder clay) and meltwater-sorted glacial sand & gravel. The erosive powers of glacial meltwater carved distinctive channels on the sandstone escarpments of north Cheshire, such as the Hazlehurst Road channel (photo 0.04) on the southern boundary of town.

Advancing ice transported large blocks of “foreign rock” – erratics – into Cheshire and these were left behind when the ice wasted away. A number of distinctive granite & volcanic erratics survive in Frodsham town, e.g. two erratics, one painted, have been relocated at the Ring o’ Bells (photo 0.05).

An extensive cover of alluvium accumulated as the flood plains and marshes of the River Mersey and River Weaver developed. The extent of Frodsham Marsh is clearly visible from the War Memorial viewpoint on Overton Hill (photo 0.06).

Thus, the dynamic geological past has contributed to both the present rock outcrop pattern as well as the physical attributes of Frodsham’s landscape.

Within the town a geological Site of Special Scientific Interest (SSSI) was notified in 1990 with the site name: Frodsham Railway and Road Cuttings and comprises 3 separate exposures of the Frodsham Beds, as shown on **Map 2.2** (Ref.3). The 2 northerly exposures were designated a Regionally Important Geological Site (RIGS) in 2005 (Ref.4).

2.3 The Landscape Context of Frodsham

Frodsham Parish covers an area of 9.25 square miles (24 square kilometres). Over the years a number of studies have been carried out on the landscape character of the Cheshire area, including the area within and surrounding the Parish of Frodsham. These include the Cheshire Historic Landscape Characterisation Project (Ref.5), the Cheshire Landscape Character Assessment (Ref.8) and the Vale Royal Landscape Character Assessment (Ref.9). In addition to these, as part of the development of the Frodsham Town Design Statement a Parish Landscape Assessment was completed. These all reveal the varied landscape of the area in which Frodsham is situated and the various geological, physiographic, natural and historical features which give the landscape its character.



**Table 2.2** below provides an overview of and shows the relationships between the landscape character types and areas recognised in Frodsham and the surrounding area identified through these different Landscape Character Assessments and how they also align with the National Countryside Character Areas of England.

**Table 2.3** shows the Historic Landscape Characterisation Groups, Types and Subtypes identified in the Frodsham Parish area through the Cheshire Historic Landscape Characterisation Project

**Table 2.2 Nesting of landscape units at national, county, borough and parish scales**

National Countryside Character Areas (Ref. 6&7)	060 Mersey Valley			062 Cheshire Sandstone Ridge			
CCC Landscape Character Types & CCC Landscape Areas in Frodsham Parish (Ref.8)	15 Mudflats and Saltmarsh MFSM1 Ince Banks	4 Drained Marsh DM1 Frodsham Marsh	13 River Valleys R1 Lower Weaver	5 Rolling Farmland RF1 Norley	5 Rolling Farmland RF8 Helsby to Frodsham	3 Sandstone Fringe SF1 Kelsall	2 Sandstone Ridge SR1 Frodsham
Vale Royal Borough Landscape Types & Vale Royal Landscape Areas in Frodsham Parish (Ref.9)	12 Estuary 12A Frodsham Score	11 Reclaimed Saltmarsh 11A Frodsham Helsby & Lordship Marsh	8 River Valleys 8C Lower Weaver Valley	4 Undulating Enclosed Farmland 4B Frodsham to Northwich Undulating Enclosed Farmland	3 Sandstone Fringe 3A Alvanley Sandstone Fringe	2 Sandstone Ridge 2A Northern Sandstone Ridge	
Frodsham Parish Landscape Character Types (Ref.10)	1 Estuary	2 Reclaimed Saltmarsh	3 Floodplain	4 Valley-side	5 Scarp Foot	6 Wooded Escarpment	7 Undulating Elevated Land

Despite differences in the descriptive names there is a high level of landscape character compatibility between the same units identified at county, borough and parish scale.

The boundaries of Landscape Character Areas within the town are a continuation of those mapped in the parish. They primarily demonstrate Frodsham’s location on the Scarp Foot of the Sandstone Ridge. At the boundary between the Lower and Upper Frodsham Foot-slope there is a distinct upward steepening of land to the south, though, this reduces to a gently sloping shelf in Overton before rising steeply to Beacon Hill.

**Table 2.3 Cheshire Historic Landscape Characterisation Groups, Types and (Subtypes) Recognised in Frodsham Parish (Ref.5)**

Ancient Fieldscapes	Post Medieval Fieldscapes	C20th Fieldscapes	Woodland	Ornamental	Water Bodies	Recreation	Settlement	Industry	Communications
Ancient Field Systems (Ancient Field Systems Semi-regular)	Late Post Medieval Agricultural Improvement (Late Post Medieval Agricultural Improvement)	C20th Agricultural Improvement (C20th Agricultural Improvement)	Ancient Woodland (Ancient Woodland)	Post Medieval Ornamental Parkland (Post Medieval Ornamental Parkland)	Natural Waterbodies (Natural Waterbodies)	Golf Course (C20th Golf Course)	Post Medieval Settlement (Post Medieval Settlement)	C20th Century (C20th Industry Active)	Post Medieval Communications (Post Medieval Communications Active)
Medieval Townfields (Medieval Townfields)	C19th Field Systems (C19th Planned Enclosure of Marsh, Parliamentary Enclosure)	C20th Century Field Systems (C20th Century Field Systems)	Plantation (Post Medieval Plantation)		C20th Artificial Water Bodies (C20th Artificial Water Bodies)		C20th Settlement (C20th Settlement)		
			Other Woodland (Post Medieval Other Woodland)						

2.4 Frodsham’s Town Landscape

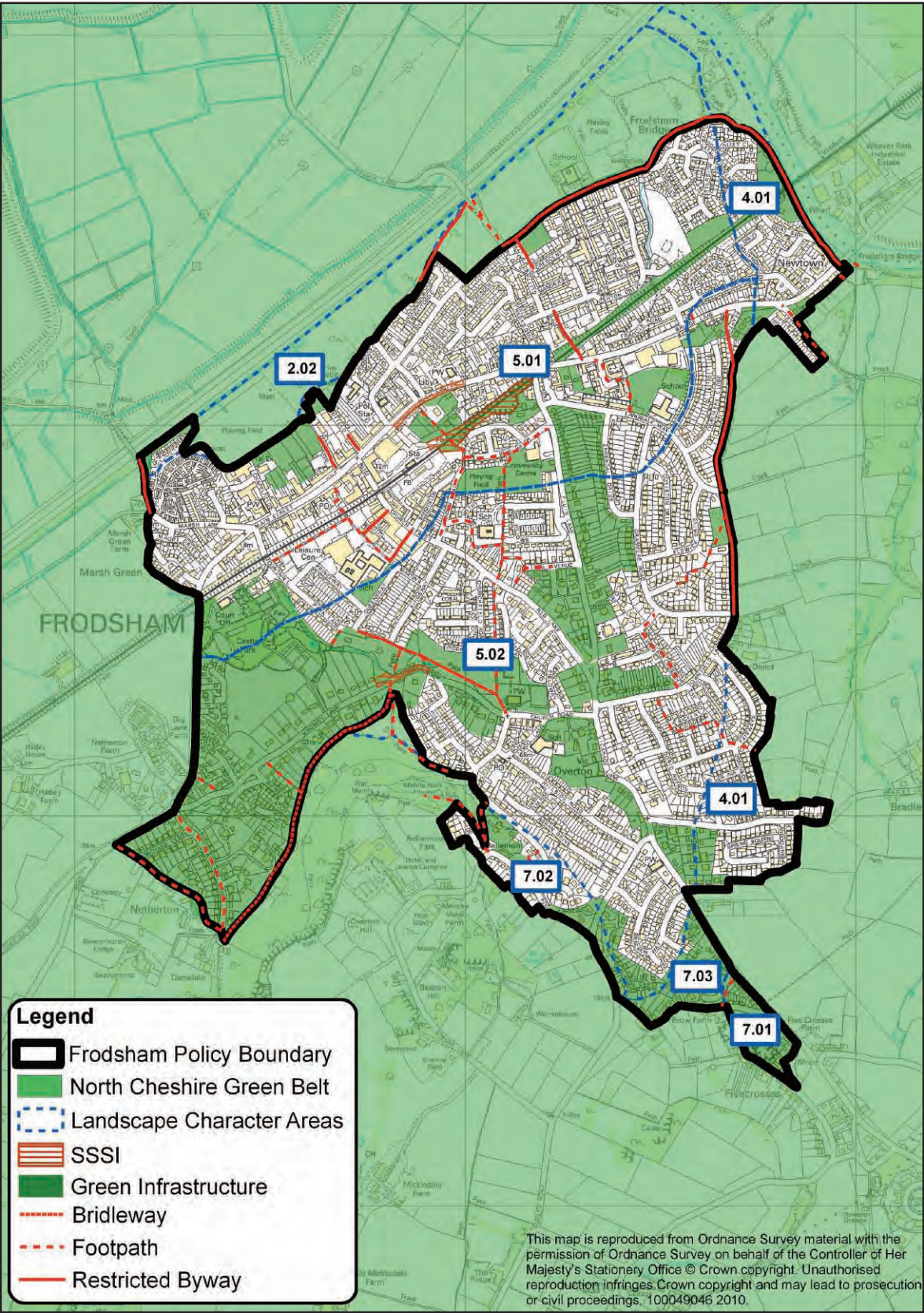
The Frodsham Parish LCA carried out by members of the Town Design Statement Team recognised 7 Landscape Character Types (**Table 2.2**) comprising 19 Landscape Character Areas within the Parish. Frodsham Town occupies only a small area of the large Civil Parish and has developed over parts of 4 Landscape Character Types and 7 Landscape Character Areas. **Table 2.4** and **Map 2.2** show the Landscape Character Areas with a presence in the town and their location.

**Table 2.4 Landscape Character Types and Areas in Frodsham’s Town Policy Boundary (Ref. 11)**

Landscape Character Type	Landscape Character Area
2 Reclaimed Saltmarsh	2.02 Frodsham Marsh Drains
4 Valley-side	4.01 Weaver Valley-side
5 Scarp Foot	5.01 Lower Frodsham Foot-slope 5.02 Upper Frodsham Foot-slope
7 Undulating Elevated Land	7.01 Manley Road Upland 7.02 Beacon Hill 7.03 Five Crosses Channel System



Map 2.2 Landscape Character Areas, Green Infrastructure, Footpaths and Bridleways in Frodsham (Ref. 3, 10 & 12)



2.5 Frodsham’s Green Infrastructure

Green infrastructure is the “life support system – the network of natural environmental components and green and blue spaces that lies within and between...cities, towns and villages which provides multiple social, economic and environmental benefits” (Ref.11). Nature conservation, habitat creation and improvement provide multi-functional green spaces that enhance the visual amenity of places and mitigate the impact of climate change, for instance, within towns green spaces help to reduce storm runoff and flooding on low-lying land, to ameliorate the effect of high summer temperatures and to assist wildlife survival by providing ‘urban’ habitats and foraging corridors.

The TDS team have recognised and mapped the Green Infrastructure within Frodsham’s TPB. It includes public parks and recreational green spaces, private gardens, the cemetery and allotments. Many of these places connect to form biodiversity corridors within the town and/or facilitate wildlife movement across the Green Belt / Town Policy Boundary. The Green Infrastructure is shown on **Map 2.2**.

2.6 Frodsham’s Public Rights of Way

A map of the Public Rights of Way (PROW) in Frodsham Parish was produced under the Parish Paths Partnership initiative “to protect and improve the local network of parish paths” (Ref. 12). Footpaths, bridleways and restricted byways within and at the edge of town are shown on **Map 2.2**. The town’s network of footpaths and residential roads provides short-cuts and encourages walking; some town footpaths link to Long Distance Footpaths such as The Sandstone Trail. In addition, there are sections of bridleway and restricted byway in the Town Policy Boundary.

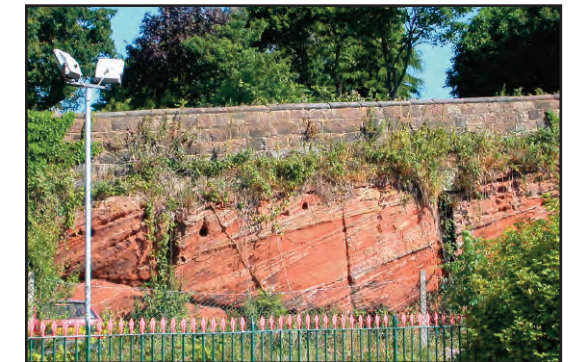


## Section 2 References

1. British Geological Survey (2004) Lexicon of named rock units, available online at [www.bgs.ac.uk/lexicon/lexicon\\_intro.html](http://www.bgs.ac.uk/lexicon/lexicon_intro.html)
2. Geological Survey (1977) Runcorn Sheet 97 (S&D) 1:50,000
3. Nature Conservancy Council (1990) Frodsham Railway & Road Cuttings SSSI notification
4. Cheshire RIGS (2002) Frodsham CH 036 Geological Site Report
5. CCC & EH (2007) Cheshire Historic Landscape Characterisation Final Report
6. Countryside Agency (2005) The Character of England – Landscape, Wildlife, Natural and Cultural Features – map
7. Countryside Commission (1998) Countryside Character, Volume 2: North West – The character of England's natural and man-made landscape CCP 536
8. CCC (2007) Cheshire Landscape Character Assessment - DRAFT
9. VRBC (2007) Landscape Character Supplementary Planning Document
10. K Gee (2008) Landscape Character Assessment of Frodsham Civil Parish - UNPUBLISHED
11. NWDA (2008) North West Green Infrastructure Guide version 1.1
12. CCC (1996) Frodsham Parish Paths



0.01 The Frodsham Fault with mottled Wilmslow Sandstone on the left and younger sandstone to the right, Howey Lane-SSSI.



0.02 Frodsham Beds representing fossilised sand dunes in the railway cutting behind the east bound platform. SSSI and RIGS site.



0.03 Tarporley Siltstones, Bridge Lane.



0.04 Meltwater channel followed by Hazlehurst Road.



0.05 Granite erratics Church Road, Overton.



0.06 Frodsham, Frodsham Marsh and the mouth of the River Weaver from the War Memorial Viewpoint



3 The development of Frodsham

3.1 Origins

The settlement of “Frodsham” dates from Saxon times and is recorded in the Domesday Book as an important agricultural manor (estate) with a small workforce plus a priest and a church. The **Early Medieval Estate Centre** was probably located in Overton close to the present-day Parish Church of St Laurence. The settlement at Overton grew under the lordship of the Norman Earls of Chester until the powerful 6th Earl – Ranulph de Blundeville – granted a Borough Charter to Frodsham, probably in 1209. The Charter led to the development of the **Planned Medieval Borough** in what is now the commercial core of the town. In addition, Earl Ranulph relocated the estate HQ to a fortified manor-house / “castle” close to the junction of the old road (Howey Lane) and a realigned Chester to Warrington road was built wide enough to hold a regular market. 110 burgage plots – distinctive, long narrow parcels of land suitable for a home, trade and an “allotment” – were laid out end-on to both sides of the new thoroughfare and rented from the Lord of the Manor for 12d (old pence!) per year. The benefits for tenants included freedom from full-time agricultural duty, and license to trade in the market place and through the new river port at Frodsham Bridge, where Newtown Medieval Settlement, grew up (Ref.1).

3.2 Settlement development and population growth

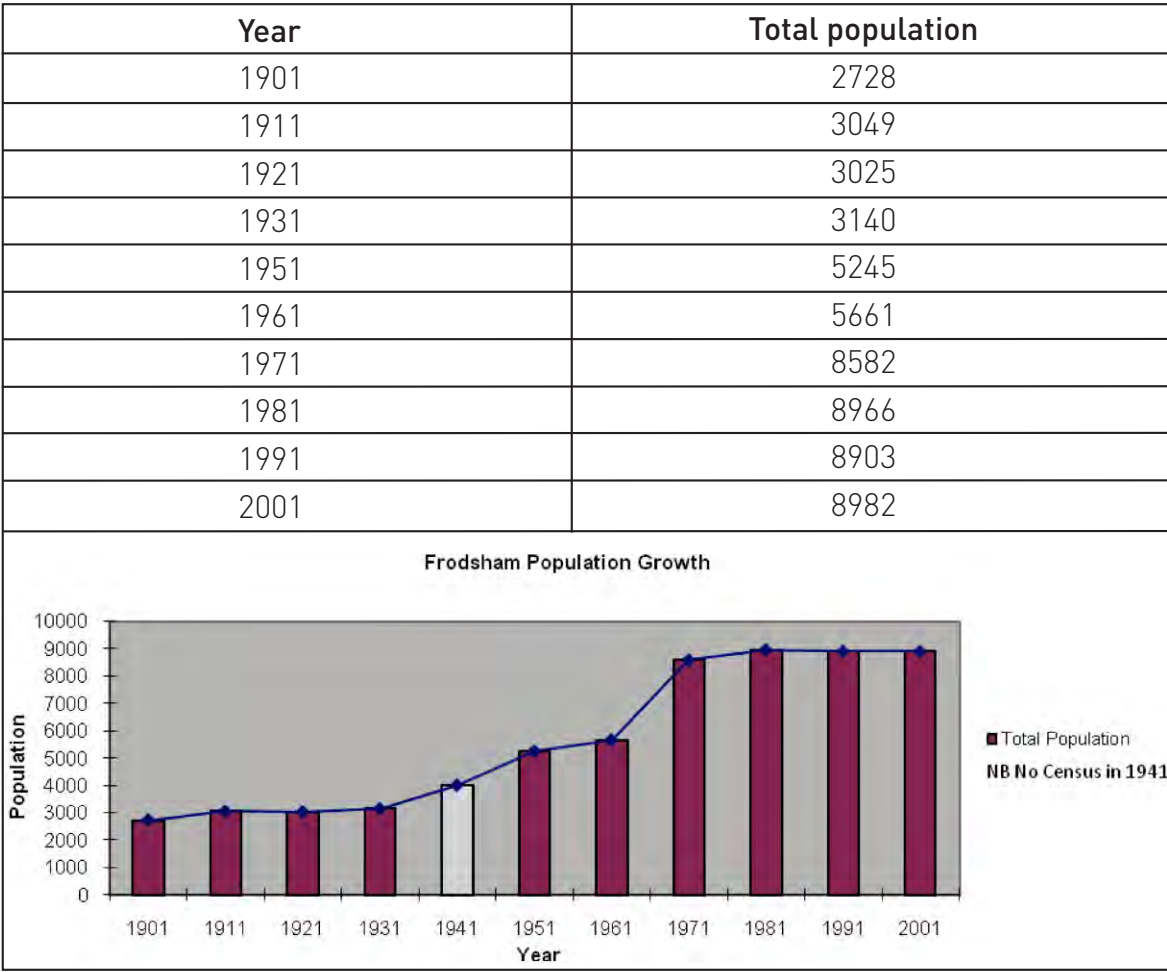
In 1237 the earldom of Chester and the manor of Frodsham reverted to the Crown and remained in royal hands for most of the later medieval period. Agriculture dominated the economy well into the post-medieval period with cheese-making, using milk from herds grazing the marshes, being important until the 1850s. Frodsham’s location gave it importance as a staging post during the coaching era: The Bear’s Paw, built 1632, was one of several coaching inns. By the 1700s a variety of craftsmen were working in the township, e.g. at weaving, tanning and shoe making. Frodsham failed to attract industry, with the exception of the small scale manufacturies including a flour mill and a salt works in Newtown. As a consequence, the township experienced limited development during the industrial era, although there is a legacy of large residential properties associated with economic success in Frodsham and beyond, e.g. Joseph Stubbs’ Park Place, 1851, developed from property on the site of the fortified manor-house and now called Castle Park House; John Urmston’s The Gables (52 Main Street), and William Crosbie’s house at 1 Church Street, were once the homes of the business partners who established the Salt Works at Newtown.

Nevertheless, right up to the late 19th century Frodsham, Overton and Newtown were still separate entities (Ref 1-5).

Infill and peripheral expansion of the settlement area during the 20th century has produced the town we know today (see 3.4 below). In the 21st century the medieval street layout is largely retained, and Main Street still hosts a weekly market (photo 0.07), but, the footprint of burgage plots shows only partial survival: mainly on the north side of Main Street and High Street.

Frodsham’s population growth mirrors the pattern of settlement development. Population grew slowly from medieval times until well into the 20th century. In 1936 boundary changes established Frodsham Civil Parish and no census was conducted in 1941. The second half of the 20th century witnessed an altogether greater rate of population growth which was most marked during the 1960s. Frodsham’s population had increased to c.9000 by the 1990s – see **Table 3.1** and the graph of population growth (Ref.6).

Table 3.1 Frodsham’s 20th century Population Growth (Refs.6)

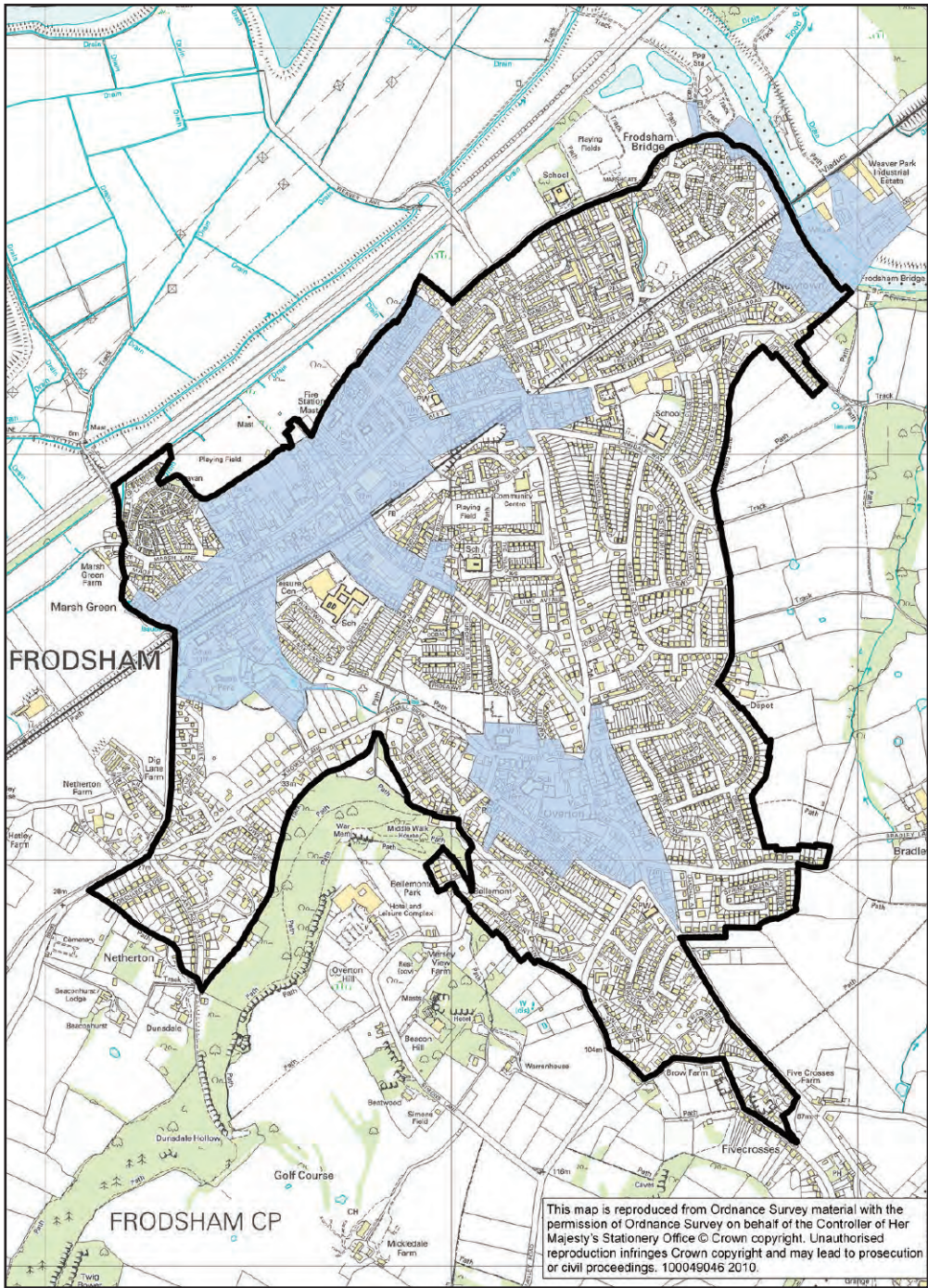




3.3 Archaeological Assessment

A systematic archaeological assessment of Frodsham was conducted as part of the Cheshire Historic Towns Survey. It identified the three areas of medieval settlement noted in paragraph 3.1 above as a single “Area of Archaeological Potential (AAP), which it further subdivided. **Map 3.1** shows the extent of the Area of Archaeological Potential in Frodsham.

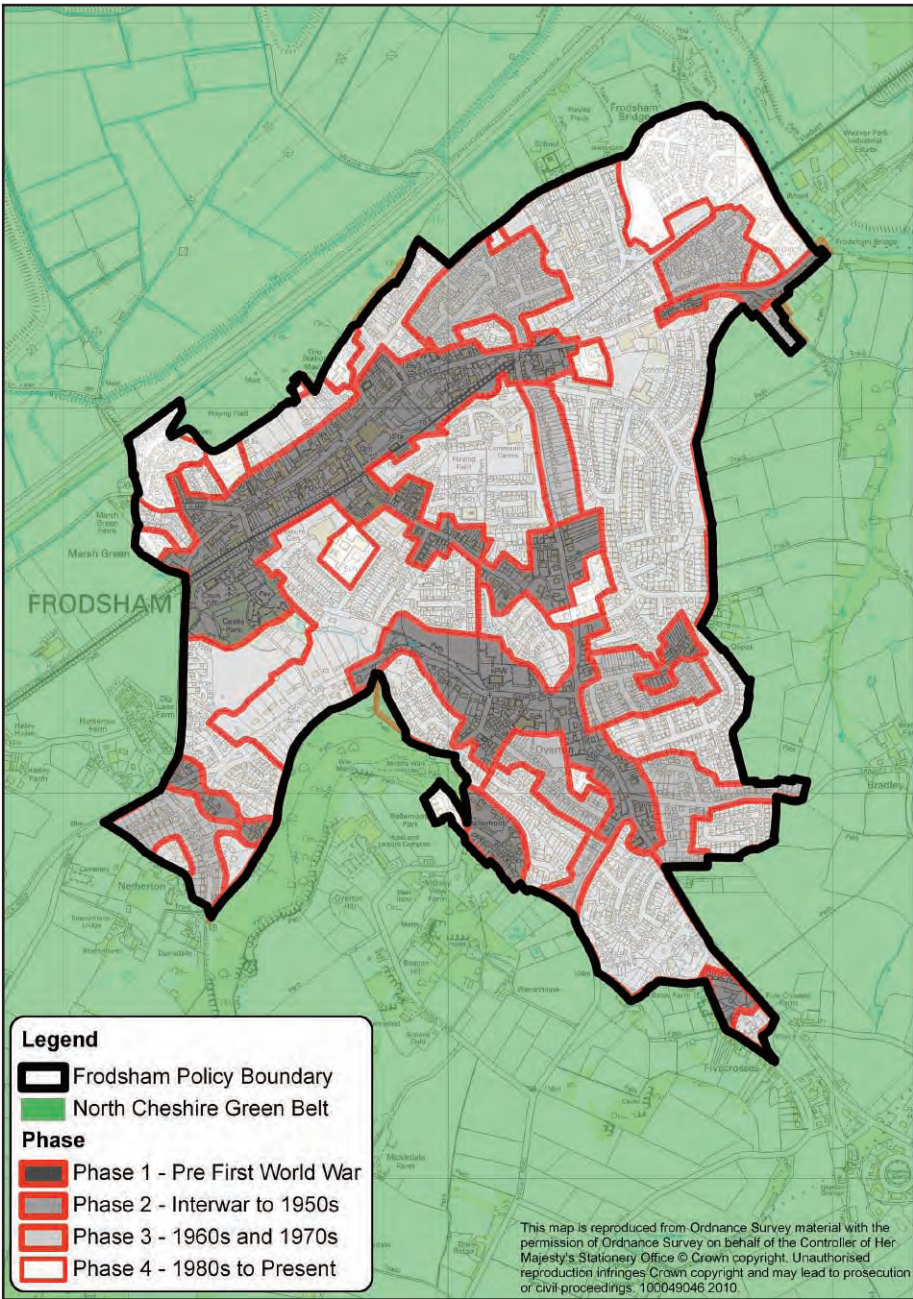
Map 3.1 Frodsham Area of Archaeological Potential (Ref.7)



3.4 Settlement infill and expansion

Many properties in the area of the Planned Medieval Borough, now more recognisable as the commercial core of the town and several in Overton, date from the 17th and 18th centuries. The oldest known property is a one-room cottage, dated to 1580, now part of The Old Cottage, on The Rock in High Street. A broad-brush approach to mapping the growth of modern Frodsham, based on observation and the documentary record of property age, identifies distinct phases and areas of development during the 20th and 21st centuries. **Map 3.2** compiled by the TDS team displays the reconstructed pattern of the infill, growth and expansion of the town.

Map 3.2 Distribution of property age groupings in Frodsham





**Phase 1. Pre-First World War** This category identifies those areas where the town's surviving older properties are mainly located, indicating the early extent of the town: it broadly corresponds with the AAP shown in Map 3.1. These properties include places of worship, inns, former farms and structures for new and improved transport as well as private homes – many of the buildings are nationally or locally listed, e.g. the Church of St Laurence, late 12th century; the Queen's Head Hotel, Main Street and the outbuildings to Manor Farmhouse, now refurbished as dwellings, both dating from the 17th century; Frodsham Viaduct and Frodsham Bridge, both completed in 1850; and a diverse range of private homes such as the 17th century thatched cottages and Ashley House, (photo 0.08) in Main Street, and Albert Row, between High Street and Ship Street (photo 0.09).

**Phase 2. Interwar to 1950s** This period saw several small housing developments take place, each with distinctive design features. This phase includes the area to the south of Main Street between Church Street and Castle Park that surrounds the Leisure Centre and recently demolished secondary school. Upper Park Lane (photo 0.10) is part of this area and Hillsborough Avenue, in Overton (photo 0.11) is also an example of this period of settlement expansion.

**Phase 3. 1960s and 1970s** These decades saw a variety of residential development on all sides of the town: the most extensive occurring to the east and south of Frodsham, e.g. Wayford Close (photo 0.12) and Fairways (photo 0.13).

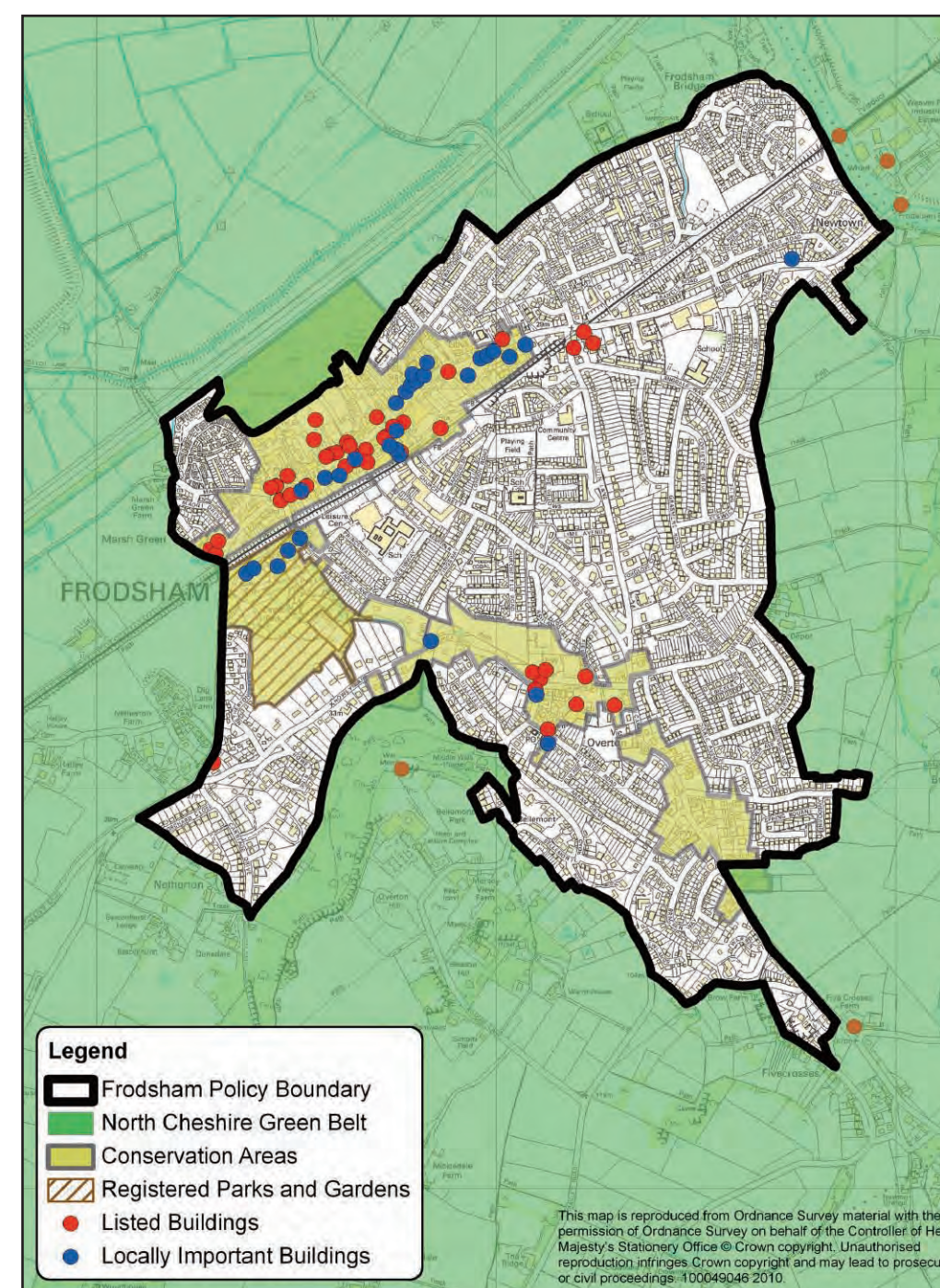
**Phase 4. 1980s to present** This period saw small areas of significant residential infill from single bungalows and houses on windfall sites to large apartment blocks on the sites of demolished property, e.g. bungalows near Lawrence House, Churchfield Road; four pairs of semi-detached houses in Ship Street near the junction with Weaver Lane; seven detached properties in Foxglove Court off Fluin Lane; Bridge Lane Mews, next to Frodsham Bridge (photo 0.14) and a mixed residential estate developed at the former Saltworks Farm on the northern edge of Frodsham (photo 0.15).

### 3.5 Heritage Assets

Within Frodsham TPB there are a range of designated heritage assets: namely, four conservation areas, 45 nationally listed buildings, 78 locally important buildings, and, 22 unlisted buildings of townscape merit in the Overton Conservation Areas; details are provided in **Appendix 3** (Ref.8-13). In addition, Castle Park is a Registered Historic Park and Garden (Ref.14).. **Map 3.3** shows the location of conservation areas, listed buildings and the registered park.

Between 2001 and 2005 borough and county authorities secured matched funding from English Heritage for the Frodsham Heritage Economic Regeneration Scheme (HERS) to support the promotion of tourism. Grants were allocated for the repair of 22 properties, the construction of three peninsular crossing points and new street furniture in Main Street. The key heritage features of the commercial core were explained with additional blue plaques and new interpretation panels (photo 0.16).

**Map 3.3 – Frodsham's Heritage Assets (Ref. 8-14)**

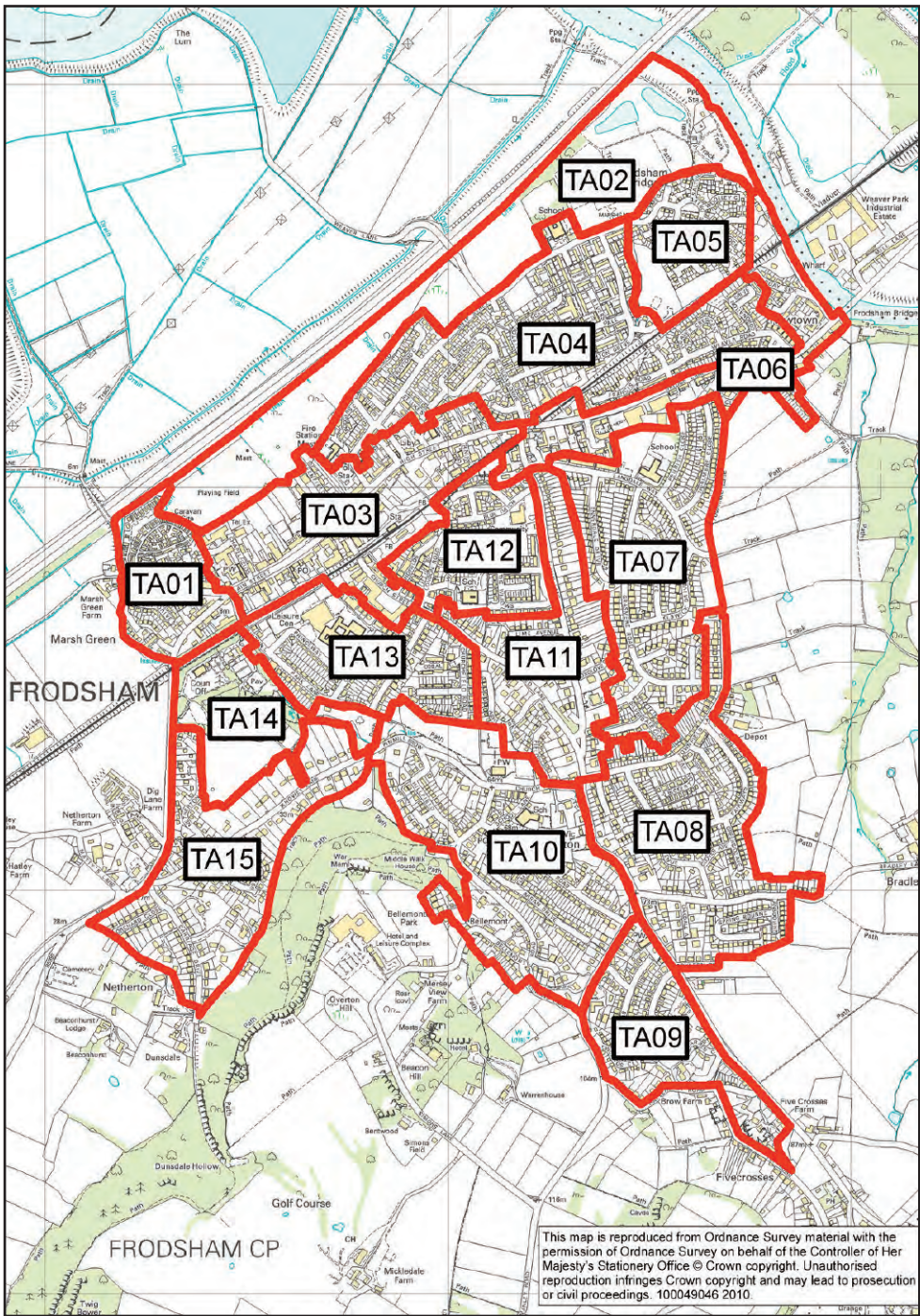




3.6 Recognition of Townscape Areas

At an early stage in the project the TDS team divided Frodsham into 15 Townscape Areas based on perceived borders created by physical obstacles such as rear garden boundaries, busy roads and the railway. **Map 3.4** shows the agreed Townscape Areas following public consultation and minor boundary adjustments made during the 2008 resurvey.

Map 3.4 – Frodsham’s Townscape Areas



Section 3 References

1. CCC (2003) *Cheshire Historic Towns Survey – Frodsham – Archaeological Assessment*
2. Cheshire Tithe maps and early OS maps online at: <http://maps.cheshire.gov.uk/tithemaps>
3. Historic statistics for Frodsham online at: <http://www.visionofbritain.org.uk/index.jsp>
4. F&DLHG (1989) *Discovering Castle Park, Frodsham*
5. F&DLHG (1985) *Discovering Old Frodsham*
6. Office of National Statistics, Census 1981-2001. Crown copyright. Crown copyright material is reproduced with the permission of the Controller of HMSO
7. CCC (2003) *Cheshire Historic Towns Survey – Frodsham – Archaeological Strategy*
8. VRBC (2003) *Frodsham (Castle Park) Conservation Area Appraisal*
9. VRBC (2005) *Frodsham (Overton, Five Crosses) Conservation Area Appraisal*
10. VRBC (2005) *Frodsham (Overton, St. Lawrence's) Conservation Area Appraisal*
11. VRBC (2006) *Frodsham (Town) Conservation Area Appraisal*
12. VRBC (2006) *Adopted Vale Royal Borough Local Plan First Review Alteration*
13. VRBC (2006) *Adopted Vale Royal Borough Local Plan First Review Alteration Proposals Maps*
14. EH Register of Parks and Gardens (2002) *Castle Park GD3508*





0.07 Thursday Market in Main Street.



0.08 Ashley House c.1830, Main Street. LB



0.15 Saltworks Farm 1990s property



0.16 Interpretation panel in Church Street (HERS).



0.09 Albert Row, a Victorian terrace in Frodsham (Town) Conservation Area. LIB



0.10 Park Lane property, interwar – 1950s.



0.11 Hillsborough Avenue property, interwar – 1950s.



0.12 Wayford Close 1960s residential property.



0.13 Fairways 1960s property.



0.14 Bridge Lane Mews 21st century residential complex







- With the exception of Main Street, residential roads carry little traffic but on-street parking contributes to traffic congestion in Marsh Lane and there is intrusive background noise from the M56 throughout the area.
- There is a panoramic view from Brook Furlong, on the northwest edge of the area, extending from Trinity Church spire in Frodsham to Helsby Hill and St. Paul's Church (View A). Overton Hill and the War Memorial can be seen from several locations in the Marsh Lane area

**Townscape character description**

- The area is predominantly residential. Many properties are brick built with pitched slate or tile roofs.
- A former builder's yard of mixed businesses is enclosed behind 19th century Brook House (LB) at the corner of Fountain Lane and Main Street (photo 01.1).
- Properties range in age from early 18th century Millbank House (LB) to 21st century Meadowside.
- Property types include terraces built during the 19th and 20th centuries, e.g. Ashley Gardens (LIB); Moreton Terrace (LIB) (photo 01.2); and Cliff View. They give parts of this townscape a distinctively compact character.
- Maori Drive and Brookside Road are well established developments of mainly semi-detached houses built during the 1960s.
- Modern residential developments include an extensive area of attractive Park Homes (photo 01.3) and the spacious 21st century development of Meadowside which extends to the Green Belt / Town Policy Boundary.
- Extensions and alterations to windows, entrances and chimneys on properties of all ages are evident and sometimes detract from the unity of terraces and streets.
- Recurring design features adding interest to the townscape include use of contrasting brick colours and decorative brickwork on property walls and chimneys (photo 01.4) and distinctive coursed sandstone boundary walls.
- The Brook Stone, a large glacial erratic mounted on a plinth at the junction of Main Street and Marsh Lane, is a distinctive street feature.

**Planning Guidance**

1. All new development should safeguard and improve the quality of the environment within and adjacent to the Conservation Area.  
(LPP: BE1; BE4; BE10; BE11; BE19; BE21; BE22; BE23)

2. New development should retain the distinctive fabric and architectural features on listed and locally listed buildings, e.g. Bourne Methodist Chapel. (photo 01.5)  
(LPP: BE1; BE4; BE5; BE6; BE7; BE8; BE9; BE19; BE21; BE22)
3. All new development should maintain and repair existing sandstone walls and copings using traditional methods and materials (photo 01.6).  
(LPP: BE1)
4. All new development should respect and enhance the setting of the Brook Stone (photo 01.7).  
(LPP: BE1)
5. All new development should retain existing views to the War Memorial and Overton Hill from residential roads (photo 01.8).  
(LPP: BE1)
6. All new development should ensure the panoramic view from Brook Furlong is protected (photo 01.9).  
(LPP: BE1)

**Community Aspirations**

- The community hopes that Brook House will be conserved (Vale Royal At Risk Strategy, score 4) (photo 01.10).  
(LPP: BE1; BE5; BE6; BE7; BE8; BE9; BE10)
- The community hopes that properties in the Marsh Lane area on the list of locally important buildings, drawn up by VRBC in 1977-79, will be re-evaluated for inclusion on a revised 21st century Local List.
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.



01.1 Small businesses re-using old buildings between Brook House and the railway embankment are largely out of sight



01.2 Moreton Terrace LIB - original brickwork, chimney stacks and slate roofs retained.





01.3 Access to the Park Homes site.



01.4 Millbank House LIB - the decorative brickwork is a common feature throughout the town.



01.5 Bourne Methodist Chapel LIB - future use must retain facade and stone frontage (cf. United Reform Church Bridge Lane, TA06).



01.6 Extensive sandstone walls on the south side of Main Street and elsewhere in Frodsham should be retained.



01.7 The Brook Stone - granite boulder - interpretation would be helpful.



01.8 View SE to Overton Hill from Meadowside.

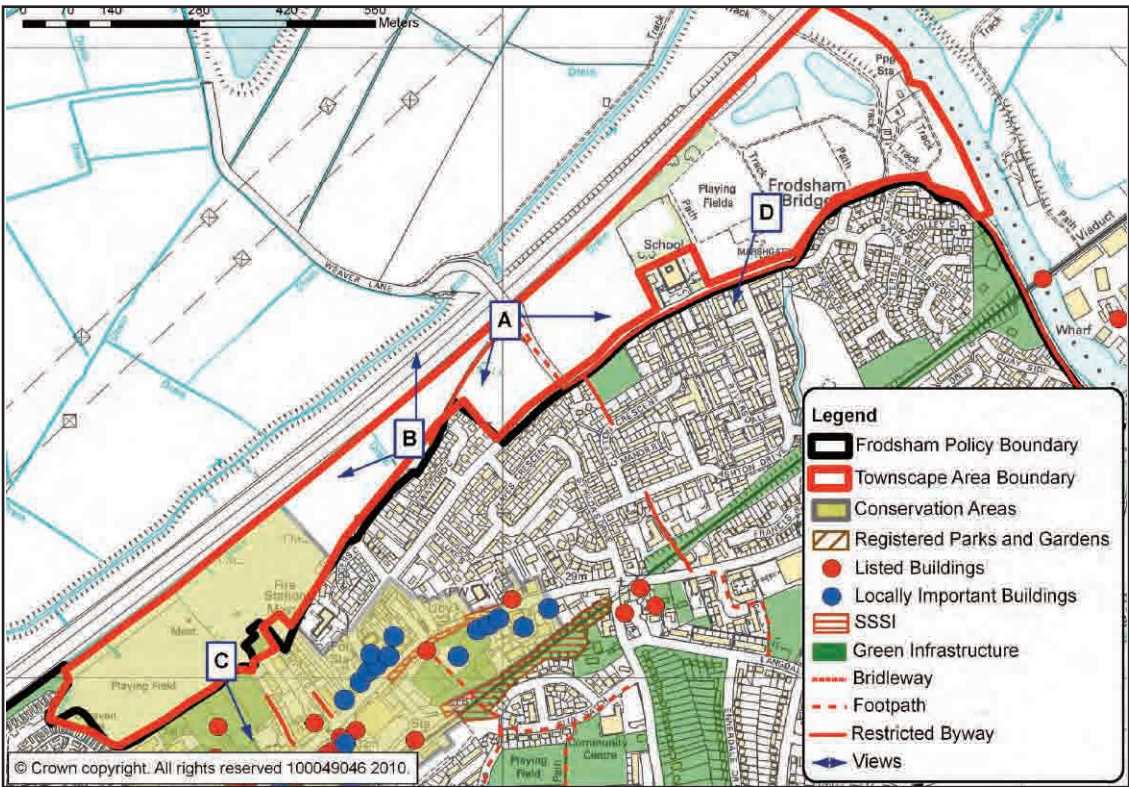


01.9 View SE to parish church from Brook Furlong.



01.10 Brook House - deteriorating LB - occupies a key site in the conservation area.

Townscape Area 02: Motorway Buffer Zone



Key characteristics

- The Motorway Buffer Zone is an area of Green Belt land between the Town Policy Boundary and the M56 which offers recreational and educational facilities for the local community.
- The area is predominantly green infrastructure comprising a mixture of recreational open space and wildlife habitat that has developed from agricultural land since the Preston Brook to Hapsford section of the M56 was opened in 1971. Parts of the area are at risk from flooding – see Proposals Map – and the southwest part lies within Frodsham (Town) Conservation Area (**Map 3.3**).
- The whole of the area is subject to ever-present, high volume, motorway noise.
- Two rural footpaths cross the area giving access to Frodsham Marsh.
- There is a fine view from Weaver Lane motorway bridge over the central area of the Buffer Zone (view A). Hawthorne Road offers views across rough ground to motorway traffic, power transmission lines on Frodsham Marsh, and the chemical works in Runcorn (view B). The War Memorial is a prominent landmark from the cricket ground (view C) and Overton Hill provides a backdrop to the view across the town from Saltworks Farm Playing Fields (view D).



**Townscape character description**

- The southwest part of the area is level ground with poor natural drainage. In contrast, the land rises to Ship Street on either side of Weaver Lane.
- One field, west of Weaver Vale Primary School and Nursery, is still in cultivation whilst others in the central section have reverted to nature, becoming wetland, rough grassland and scrub. Relict hedgerows indicate former field boundaries. These areas are diverse in character and provide good wildlife habitats (photo 02.1).
- The playing field in the southwest, formerly used by Frodsham College is now used only by local cricketers. The site is surrounded by mature hedges, and is entered through metal security gates. It contains a substantial brick and tile pavilion with shuttered windows erected by Frodsham Cricket Club.
- Marshlands Tree Garden, planted on the site of the old gasworks by the local volunteer Tree Group, is a wooded area with pathways.
- Saltworks Farm Playing Fields provides extensive community facilities for skateboarding and team sports – primarily football.
- A substantial brick and tile utilities building is sited near the entrance to Saltworks Farm Playing Fields.
- The area in the angle between the motorway viaduct and the west bank of the River Weaver, containing 2 large excavated ponds, is managed for wildlife and recreational pursuits (photo 02.2). It can be reached by a rough track close to the river which leads to a nearby United Utilities plant.

**Planning Guidance**

1. All new development should protect the role of Green Belt designation.  
(LPP: GS3)
2. All new development should protect and enhance the green infrastructure for a range of recreational uses at Saltworks Farm Playing Fields.  
(photo 02.3) (LPP: BE1; RT4)
3. All new development should protect and enhance Marshlands Tree Garden, and land to the northeast, for public green space and wildlife habitat.  
(photo 02.4; 02.1) (LPP: NE1; NE4; NE7; BE1; RT4)

4. New development should ensure the sloping green space and cultivated field either side of Weaver Lane are retained as part of the biodiversity corridor parallel to the M56.  
(photo 02.5) (LPP: NE1)
5. All new development should protect the views from Weaver Lane motorway bridge, the cricket ground and Saltworks Farm.  
(photo 02.6) (LPP: BE1)

**Community Aspirations**

- Local people hope that efforts will be made to reduce and contain motorway noise to protect and enhance the educational and recreational facilities in this area of Green Belt.
- Local people wish to retain and improve the nature conservation resource of the Motorway Buffer Zone through planting of native trees and shrubs at the motorway boundary where feasible.  
(photo 02.7) (LPP: NE1; NE7; NE8; BE1)
- The community hopes that the playing field, in the Conservation Area, remain a viable recreational greenspace.  
(photo 02.8) (LPP: RT4)
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.



02.1 Wetland vegetation and a relict hedgerow in the central section.



02.2 One of the excavated ponds in an area used for fishing, bird watching and dog walking.





02.3 Skateboard park and football pitches at Saltworks Farm Playing Fields.



02.4 Entrance to Marshlands Tree Garden.



02.5 Fields on either side of Weaver Lane.



02.6 View SSW to Beacon Hill and Overton Hill from Weaver Lane motorway bridge.

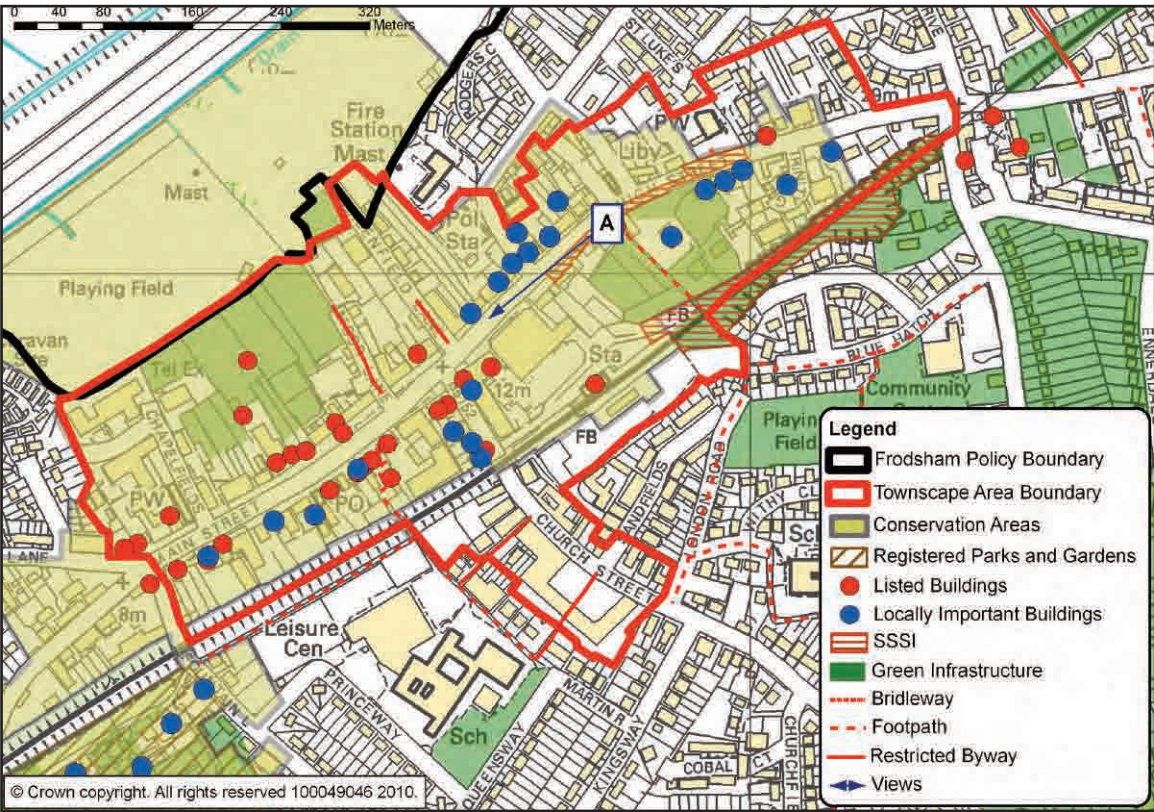


02.7 Impoverished wildlife habitat and no acoustic barrier adjacent to motorway.



02.8 Former school playing field in the Conservation Area used by Frodsham Cricket Club.

Townscape Area 03: Commercial Core



Key characteristics

- The Commercial Core occupies the site of the 13th century Planned Medieval Borough. The area is located between Castle Park and Marsh Lane Townscape Areas in the west and the Frodsham Bridge Townscape Area to the east.
- The area's key roads are Main Street, High Street and the northern half of Church Street where the layout of the former borough is still evident.
- Almost all of the area lies within the Frodsham Area of Archaeological Potential and Frodsham (Town) Conservation Area; it contains 28 listed buildings and 39 locally important buildings. **(Appendix 3)**
- The sandstone exposures northeast of the railway station, and on High Street and The Rock, form part of the Frodsham Railway and Road Cuttings Site of Special Scientific Interest (SSSI).
- The area is a vibrant and visually attractive town centre with a range of small businesses including shops / hairdressers, banks / property services, cafes / restaurants and public houses, many of which are family run. Narrow lanes and alleys lead to back-land development of variable quality, and a variety of open spaces, on the site of former burgrave plots.



- The A56 is routed along Main Street and High Street and heavy through-traffic causes congestion during busy periods of the day.
- There is a good view from The Rock through High Street and Main Street to Helsby Hill (view A and front cover). There are views in all directions from Frodsham Station footbridge and glimpses of the marshes, mouth of the River Weaver and chemical industry in Runcorn between residential properties at the eastern end of High Street.

### ***Townscape character description***

- The public face of the Commercial Core is a diverse mix of buildings mostly with domestic origins; the ground floors of many have been converted for use as shops and services (**photo 03.3**). In contrast and out-of-sight, back-land development includes small industries, community meeting places, private car parks and wilding green spaces (**photo 03.23**). Towards the east and west the area becomes increasingly residential.
- Main Street is broad and tree-lined with provision for parking (**photo 03.1**); it has prominent traffic control furniture at its junction with Church Street and 4 pairs of peninsula crossing points between Church Street and Fountain Lane. The north side of Main Street is transformed every Thursday into a busy street market which attracts shoppers from a wide area.
- The character of the core owes much to its surviving historic buildings and their use of contrasting building materials and styles. Among the larger properties are The Bear's Paw (LB), a former sandstone coaching inn; 'grand' Georgian buildings such as Ashley House (LB) and Lloyds TSB (LB), formerly the Town Hall (**photos 03.9; 0.08; 03.19**) and the imposing Queen's Head (LB) with a rendered frontage. 105-107 Main Street, formerly the Commercial Hotel (**photo 03.4**) is typical of several Victorian brick buildings that display locally distinctive detail. The smaller, but no less prominent, historic buildings include The Old Cottage (1580), on The Rock (LB), the Thatched Cottages (LB) and nos. 91-99 (LIB) (**photos 03.18; 03.24; 03.14**) – all residential – and timber-framed no.68 Main Street (LIB) – a shop (**photo 03.11**).
- 20th and 21st century development is an integral part of the mix in the Commercial Core. Large and prominent buildings include the supermarket fronting High Street and the Charter Homes development in Main Street (**photo 03.7**). Smaller scale, but still very noticeable, are Barclays Bank, next to The Bear's Paw and the development on Church Street at its junction with the station approach (**photo 03.5**).

- Most late 20th and 21st century residential development is less locally distinctive, e.g. houses and bungalows off High Street including Alexandra Mews and Rock Drive and off Main Street including Ashley Court and The Frodsham Care Home.
- Church Street, an extension of the Commercial Core, is constricted by sandstone buttresses supporting the railway bridge. The station and the listed Jacobean-style stationmaster's house is accessed from the eastern side of Church Street. South of the bridge, Church Street rises gently to Eddisbury Square, a recently refurbished 1960s shopping development with over-the-shop living accommodation that is very different in character from its historic surroundings (**photo 03.6**).
- Within the core some older, locally listed properties, despite losing their original purpose, add character and interest to the townscape e.g. the former Police Station (now a shop) and the former Court House (now Girl Guiding HQ).
- The recent neo-Georgian development, Trinity Gardens, in High Street enhances the setting of the retained landmark spire and ruins of Trinity Methodist Church, (**photo 03.8**) in stark contrast to Rock Court which is out of character with adjacent residential property, including Rock Villa, 67 High Street, a Listed Building in the Conservation Area.
- There are out-of-sight, modern residential developments on infill plots behind older properties on both sides of High Street, e.g. detached properties in large plots off Plumpstone Lane and St James Close, and behind the Knoll Surgery.
- Fisherman's Cottages (LIB) on The Rock, 28-32 High Street, have been individually refurbished with a loss of some original unifying detail.
- A metal framed sandstone obelisk outside The Bear's Paw and close to the Millennium Clock, marks the start of the Sandstone Trail, the 34 mile Long Distance Footpath to Whitchurch, Shropshire (**photo 03.9**).
- Street furniture includes a rare K4 telephone kiosk (**photo 03.10**) in Church Street. Main Street is enhanced by tall swan-necked lamp standards, plus finger post direction signs and heritage interpretation panels, a legacy of the HERS project – see section 3.5.
- Footpaths facilitate access to adjacent areas e.g. Footpath 110 from the station car park to TA12 Community Centre and Footpath 56 from Main Street to TA13 Kingsway.



Planning Guidance

- 1. All new development should safeguard and improve the quality of the environment within and adjacent to the Conservation Area.  
(LPP: BE1; BE4; BE10; BE11; BE19; BE21; BE22; BE23; H4; H5)
- 2. All new development should promote high quality building design which respects and enhances the layout of the former medieval borough, especially the surviving footprint of burgage plots on the north side of Main Street and High Street.  
(LPP: BE1)
- 3. All new development should promote high quality building design which respects and enhances the traditional building styles and local distinctiveness such as the decorative and polychrome brickwork common in the area (photo 03.2; 03.4).  
(LPP: BE1)
- 4. All new development should respect and complement existing building materials; this should not however prejudice high quality and contextually-designed contemporary development (photo 03.8).  
LPP: BE1; H4; H8; H10)
- 5. New development should sensitively conserve and adapt redundant heritage assets for new uses, where a change of use is deemed to be appropriate, e.g. the former Wesleyan Chapel (Public Library) (photo 03.15).  
(LPP: BE1; BE22)
- 6. All new development should safeguard and conserve listed buildings and their curtilage, e.g. the locally distinctive railings and/or paving at The Bear's Paw, Old Hall Hotel and 52 Main Street (photo 03.17).  
(LPP: BE1; BE5; BE6; BE7; BE8; BE9)
- 7. All new development that alters property to meet the needs of particular retail or commercial uses should maintain the character of the original building (if known), harmonise with the existing building and have respect for neighbouring properties (photo 03.11; 3.12).  
(LPP: BE1; ST5; ST6)
- 8. All new development should seek to maintain the permitted proportion of A1 uses in the retail area (60%) and to resist loss of A2 and A3 units to retain the vitality of the town centre.  
(LPP: ST1; ST8; ST9; ST11)
- 9. All new development should employ high standards of design for signage and advertising (photo 03.13)  
(LPP: BE1; BE16; ST5)

- 10. All new development should maintain and enhance existing dwellings in the town centre (photo 03.14) and promote residential use of upper floors.  
(LPP: ST4; ST10)
- 11. All new development should maintain and repair existing sandstone walls, copings and gate piers using traditional methods and materials (photo 03.16).  
(LPP: BE1)
- 12. Development should protect and interpret the geological exposures on High Street, The Rock and near the Railway Station (photo 03.18).  
(LPP: NE2; NE7; BE1)
- 13. New and replacement street furniture should be well sited and complement, but not clutter, the town centre conservation area (photo 03.19).  
(LPP: BE1; BE10; BE11; BE23)
- 14. All new development should protect and manage the lime trees, originally planted in 1897, in Main Street, High Street and Church Street (photo 03.21).  
(LPP: NE9; BE1; BE10; BE11; BE23)
- 15. Development should protect and manage back-land green spaces, some in flood risk areas, to enhance the town's green infrastructure, its biodiversity value and as a link to Townscape Area 02 (photo 03.23).  
(LPP: NE7; NE15; BE1)
- 16. All new development should protect the view from The Rock through the historic commercial core to Helsby Hill.  
(LPP: BE1) (Front cover photo)

Community Aspirations

- The community hopes that listed buildings at risk will be monitored to ensure their integrity, and restoration promoted, e.g. Frodsham Railway Station - (photo 03.20).  
(LPP: BE5; BE6; BE7; BE9)
- Local people desire the area's urban footpaths to be maintained and promoted as traffic free routes. (photo 03.22) (LPP: T8)
- The community would like to see appropriate signage to public car parks.  
(LPP: BE16)
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines





03.1 View NE along Main Street from the Marsh Lane junction



03.2 Polychrome brick façade, Red Lion (LIB), High Street.



03.9 Start of the Sandstone Trail at The Bear's Paw, 1632 (LB), Main Street.



03.10 The 'Vermillion Giant' – K4 Kiosk (LB), Church Street.



03.3 Old Hall Hotel (LB), Main Street converted from 2 cottages.



03.4 105-107 Main Street, alteration of window detail on ground and first floors.



03.11 68 Main Street (LIB) – shop window echoes surrounding window design.



03.12 Shop front (right) has disregarded local character, Church Street.



03.5 20th century development, Church Street contrasts starkly with neighbouring property.



03.6 Eddisbury Square, off Church Street.



03.13 Attractive pub sign, Church Street.



03.14 Distinctive residential property, 91-99 Main Street.



03.7 Charter Homes development off Main Street.



03.8 Trinity Gardens and retained landmark spire (LIB).



03.15 Public Library in former Wesleyan Chapel.



03.16 Locally distinctive sandstone wall in need of maintenance.





03.17 locally listed paving enclosed by railings at 52 Main Street (LB).



03.18 The Old Cottage (LB) and exposures of Frodsham Beds (SSSI), High Street.



03.19 Lloyds TSB (LB) with unfortunate siting of bus shelter, Main Street.



03.20 Unused Jacobean style station building, 1849 (LB).



03.21 Lime trees and steps enhance the character of High Street.



03.22 Refurbished Footpath 110 provides access to areas east of the railway station.

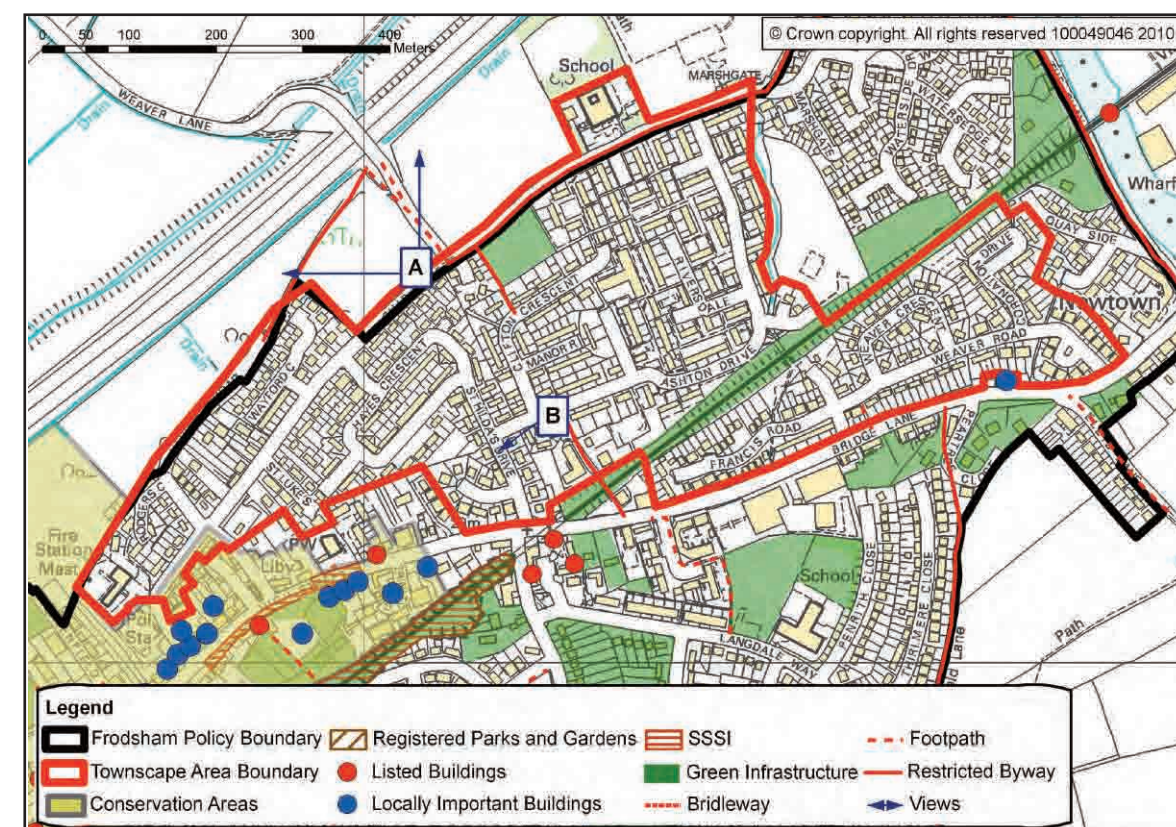


03.23 Pear tree in wilding orchard, Moor Lane.



03.24 The last remaining thatched cottages in Frodsham.

## Townscape Area 04: Ashton Drive



### Key characteristics

- Ashton Drive is the key access road in the area: it links Ship Street to the A56, via St Hilda's Drive; and pedestrian access beneath the Chester to Warrington railway links Ashton Drive to the southern part of this area.
- The Ashton Drive area lies on the northern edge of Frodsham and is bisected by the railway line: the larger part, north of the railway, has vehicular access from Ship Street and St Hilda's Drive; whilst the southern part is reached from Volunteer Street.
- Small parts of Ashton Drive lie in the Frodsham Area of Archaeological Potential and Frodsham (Town) Conservation Area. There are 4 locally important buildings on Ship Street and Hawthorne Road in the western part (**Appendix 3**).
- Ship Street carries much traffic as it is the main route to many residential properties, services and facilities, e.g. the Police Station; Fire Station; PO Sorting Office; Frodsham Weaver Vale School and Nursery; and recreation provision at Saltworks Farm.
- Constant background noise from the M56 is intrusive, especially in the northern part, and intermittent train noise, during day and night, affects nearby properties.



- There is a panoramic view from Ship Street across the motorway and marshes towards Merseyside, Liverpool Airport and the chemical works in Runcorn (view A). Trinity Church spire provides a focal point in the southwest from the brow of Ashton Drive (view B).

### ***Townscape character description***

- The Ashton Drive area is mainly residential with 2 small nodes of shops and services (photo 04.1); Meadow Court Day Care Centre; and a large children's playground on the south side of Ship Street.
- The oldest dwellings in the area are 59 Ship Street, 1784 (LIB) (photo 04.2) and Pear Tree Cottage (LIB), now derelict, on Hawthorne Road. Victorian / Edwardian terraced properties occupy the west end of Ship Street and generate onstreet parking in the conservation area (photo 04.5).
- The area has grown in discrete phases since the mid 20th century and each development retains much of its original character, e.g. among the older developments are the tile-hung 'Cornish' houses on Ship Street and Hayes Crescent; distinctively styled properties on Ashton Drive and Clifton Crescent; and the more formally planned Coronation Drive properties surrounding a small rectangular green planted with trees (photos 04.6; 04.7; 04.8).
- Frodsham Fire Station on Ship Street, close to the junction with High Street, has associated housing and a children's playground in nearby Rogers Close.
- Within, or close to the older estates are some small, more recent infill developments, e.g. the Ship Street cul de sac opposite Weaver Lane; St Luke's Way (photo 04.3); and Ashton Court.
- Around the eastern end of Ashton Drive there is a large area of 1970s development in a wide variety of property types on short spur roads, e.g. terraced houses; 3-storey apartment blocks (photo 04.4); and small bungalows for the elderly.
- Most of the older properties in this area lack provision for cars which contributes to on-street parking congestion. Within the newer developments parking is in road-side bays and blocks of garages.
- South of the railway most of the development is late 20th century detached and semi-detached housing with vehicular access via Volunteer Street from Bridge Lane.

- Older properties have small enclosed gardens but much of the newer development north of the railway is open plan design with green areas at road junctions and grass-lined pedestrian access between rows of houses and apartments.
- There is pedestrian access from Francis Road to Bridge Lane and from Weaver Road to Bridge Lane and Quayside.

### ***Planning Guidance***

1. All new development should safeguard and improve the quality of the environment within and adjacent to the Conservation Area.(photo 04.5) (LPP: BE1; BE4; BE8; BE10; BE11; BE19; BE21; BE22; BE23)
2. All new development should respect and enhance the character of distinctive property styles and their surroundings, e.g. many 'Cornish' houses in Ship Street have been clad in brick to improve wall insulation which has greatly improved their appearance. Alterations to the facades and rooflines in Clifton Crescent should be resisted to retain the visual character of the road (photo 04.6; 04.7). (LPP: BE1; BE10; BE11; BE19; BE21; BE22; BE23; H8)
3. All new development should protect and maintain recreational open spaces, e.g. the Ship Street Playground, and the community green spaces across the area, e.g. in Coronation Drive (photo 04.8) and near both ends of Ashton Drive. (LPP: BE1; RT3; RT4)
4. All new development should provide pedestrian access to facilities and adequate off-road parking. (LPP: T8; T13)
5. All new development should protect the panoramic view from Ship Street to the north across the North Cheshire Green Belt (photo 04.9). (LPP: GS3; BE1)

### ***Community Aspirations***

- Local people desire the area's urban footpaths to be maintained and promoted as traffic free routes. (photo 04.10) (LPP: T8)
- The community hopes that properties in the Ashton Drive area on the list of locally important buildings, drawn up by VRBC in 1977-79, will be re-evaluated for inclusion on a revised 21st century Local List.
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.





04.1 Ashton Drive shopping node



04.2 Rose Cottage 1784, Ship Street, LIB



04.3 St Luke's Way



04.4 1970s residential development and parking bays, Ashton Drive.



04.5 Altered terraced properties (3 LIBs), Ship Street, in the conservation area.



04.6 1950s 'Cornish' houses in Ship Street.



04.7 Distinctive terraced property in Clifton Crescent



04.8 Formal green space in Coronation Drive



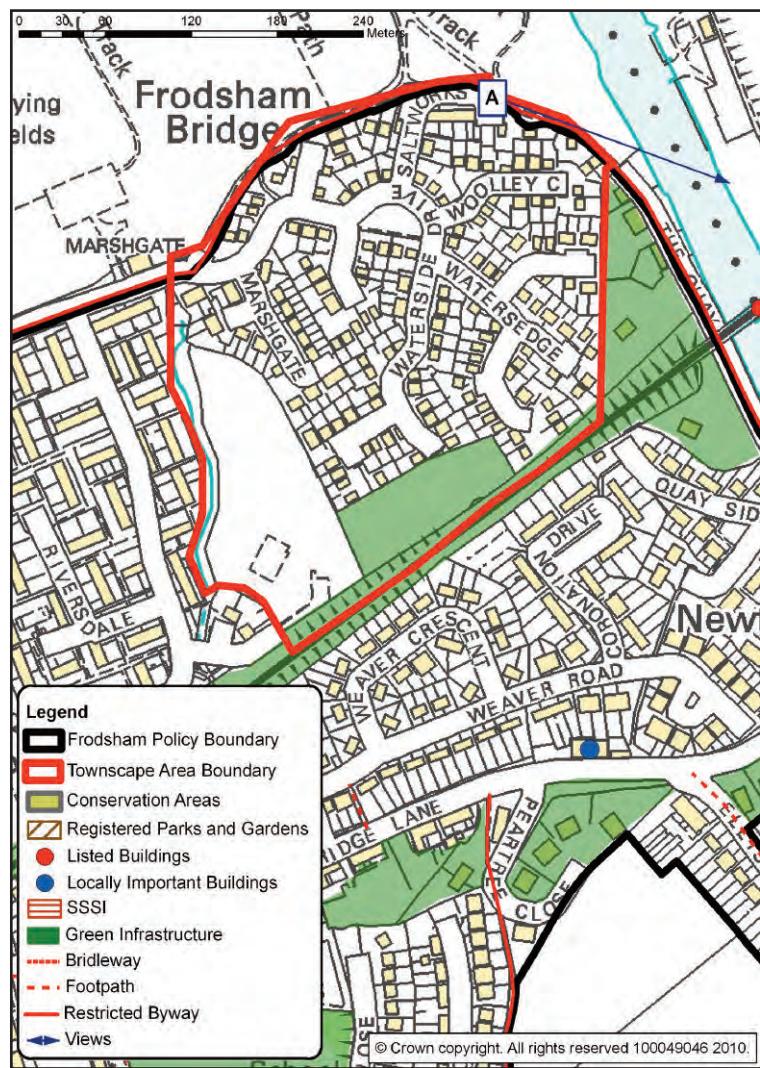
04.9 The open view to the north from Ship Street.



04.10 Pedestrian access from Ashton Drive to Volunteer Street.



Townscape Area 05: Saltworks Farm



Key characteristics

- This most northerly townscape area was developed in the 1990s on the former site of Saltworks Farm.
- The area is entirely residential and bounded by the Chester to Warrington railway in the south, National Cycle Network Route 5 to the north and east, and an open ditch to the west which separates Saltworks Farm from the Ashton Drive Townscape Area.
- There are no through roads so traffic is locally generated, however, there is intrusive background noise from the M56, especially affecting properties on the northern edge and intermittent train noise, day and night, affects parts of this area.
- There is a view from Saltworks Close across the River Weaver to the listed sandstone railway viaduct (view A). Trinity Church spire, the motorway flyover and the chemical works in Runcorn can be seen from several locations in this area.

Townscape character description

- Detached houses in a variety of styles predominate along the curving principal road with semi-detached and terraced properties (photo 05.1) common on the cul-de-sac spurs.
- Houses are built in brick, some with small rendered panels above windows, and with tiled roofs.
- Some properties include detached, attached or integral garages whilst a significant number of houses have off-road parking provision.
- Recurring design features include some decorative brickwork, hung tiles and a large number of bays and gables.
- The spur roads have no pavements but are characterised by block-paved surfaces (photo 05.2).
- There is a small green, informally planted with trees and shrubs, and most of the gardens are open plan.
- To the south and west is a large residential development where St Luke's School once stood.

Planning Guidance

1. All new development should seek to maintain the special character created by mixing private and social housing provision in TA05 (photo 05.1 and 05.2). (LPP: BE1; BE4; H4; H5; H15)
2. All new development that includes extensions and alterations to existing dwellings should maintain the character of the original building, harmonise with the existing building and have respect for neighbouring properties. (LPP: BE1; H4; H8)
3. All new development should protect and enhance the small green at the centre of this townscape area (photo 05.3). (LPP: BE1; RT4)
4. All new development should ensure the public view from PROW 84 / NCN Route 5 to the listed sandstone railway viaduct in the east is maintained (photo 05.4). (LPP: BE1; T10)
5. Maintain safe bridleways for shared use by horseriders, pedestrians and cyclists. (LPP: T8; T9)



Community Aspirations

- The community hopes that part of the former St Luke’s School site (0.16 Ha) is developed for use as recreational open space.  
(LPP: RT2)
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines



05.1 Terraced property with decorative brickwork.



05.2 Block paved cul-de-sac.

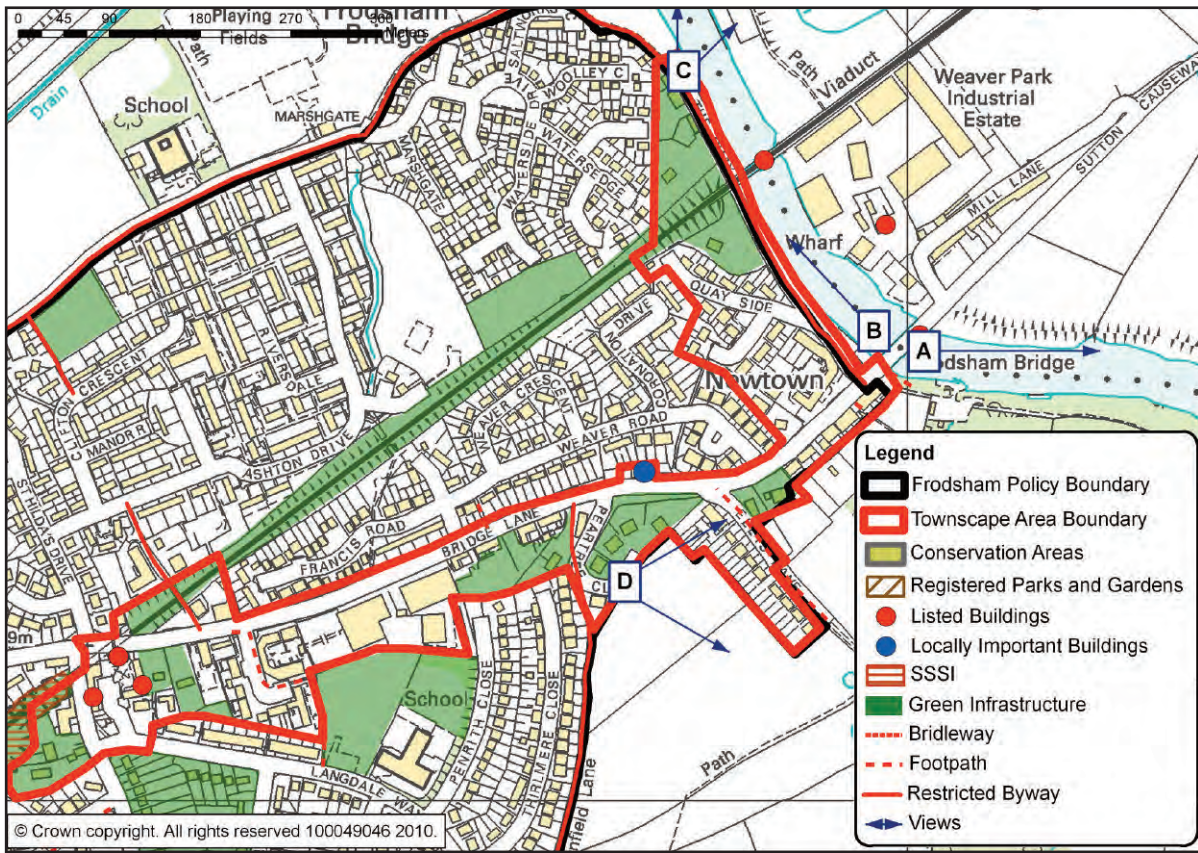


05.3 Informal green space in Waterside Drive.



05.4 View ESE to Weaver Sailing clubhouse and sandstone railway viaduct (LB).

Townscape Area 06: Frodsham Bridge



Key characteristics

- Frodsham Bridge takes its name from the lowest bridging point on the River Weaver and the port for the medieval borough.
- Frodsham Bridge Townscape Area reaches the Town Policy Boundary at the river’s edge in the east and the Green Belt/Town Policy Boundary in the south. The area extends westwards along Bridge Lane to its junction with Fluin Lane.
- The eastern part of the area is included in the Frodsham Area of Archaeological Potential. The present sandstone / limestone bridge built in 1850 carries the A56 over the River Weaver. The road bridge and the sandstone railway viaduct opened in 1850 are both listed buildings, as are 3 buildings associated with Manor Farmhouse in the west. In addition there are 4 locally important buildings close to Manor Farmhouse (Appendix 3).
- Port facilities on the Frodsham bank of the River Weaver have disappeared and the area’s buildings exhibit a diverse range of styles, ages and uses.



- The A56, routed along Bridge Lane, is busy and noisy throughout the day. In addition some parts of the area are adversely affected by motorway noise and intermittent noise from the railway and the Weaver Park Industrial Estate.
- There are good views up and down the river from the road bridge (views A & B), attractive views across the river from the north end of The Quay (view C) and an extensive view of the Weaver Valley from the top of Pear Tree Close (view D).

### ***Townscape character description***

- Frodsham Bridge Townscape Area is characterised by its domestic / commercial mix concentrated along the south side of Bridge Lane.
- Much of the north side of Bridge Lane is fronted by a high brick wall or wooden fencing. Only those properties with direct access to this side of the road are included in the area.
- The largest and most dominating commercial properties in Bridge Lane are the filling station, the adjacent block of business premises (**photo 06.1**), the Chinese restaurant and Frodsham Water Sports with its extensive structures along the river-front. Several small businesses are also based in property on Bridge Lane.
- Fraser House, a Georgian style property on Bridge Lane, between East Dene and East Bank, has been converted to offices without compromising its external appearance (**photo 06.2**).
- Residential properties in the area cover a wide range of ages and styles from 17th century farm buildings to 21st century town houses and apartments (**photo 06.3**).
- The conserved Manor Farmhouse coach / cart shed, stable / haybarn and shippon, converted to residential units (**photo 06.4**), retain their farmyard layout and vernacular features.
- There are old properties on 3 sides of the Manor Farmhouse complex, i.e. 1 and 1A Bridge Lane, a sensitively altered cottage to the north; 2 detached properties set in large grounds to the east; and, a 3-storey terrace and former chapel to the west. Two modern properties, a bungalow and a recently completed house on the site of a demolished building lie to the southeast with access from Langdale Way.

- There are several Victorian and Edwardian terraces on Bridge Lane, e.g. Barley Groves built in 1879, and the arts and craft style is represented by East Bank and East Bank Cottage (**photo 06.5**), both designated LIBs.
- Much of the terraced housing is brick built with attractive decorative design features to facades, gables and chimneys; some have sandstone walls enclosing small front gardens. In contrast, the long terrace next to the road bridge is finished in render.
- The former United Reform Church (LIB), built in 1886, has been converted into apartments whilst retaining its external character (**photo 06.6**).
- Ellis Lane is a row of mainly interwar semis with enclosed gardens on the west side of the unadopted road. Between the houses there is an access track to a field in the Green Belt that has 3 sides abutting the Town Policy Boundary.
- The Quay is a riverside track and cycle way (NCN Route 5) with access to 5 rebuilt or refurbished residential properties in the AAP (**photo 06.7**).
- Pear Tree Close is a small modern development of detached houses with open-plan gardens on an elevated site to the south of Bridge Lane overlooking the Weaver Valley.
- There is a small, modern open-plan estate of houses and flats on Quay Side plus more recent and more imposing developments of apartments and town houses at Bridge Lane Mews and Bridgewater Close.
- Small green spaces form part of the Quay Side and Bridgewater Close developments though there is little planting associated with Bridge Lane Mews.
- Good outcrops of Tarporley Siltstones exist on both sides of Bridge Lane in the vicinity of Pear Tree Close and Ellis Lane (**photo 0.03**).
- To the east of Pear Tree Close the medieval salt route, now a hollow way and footpath from Townfield Lane in TA07, meets Bridge Lane on its route to the former river port.
- The cast iron milepost on Bridge Lane near the junction with Fluin Lane, is a distinctive roadside feature (see the similar milepost in TA15).
- A former public path under the western arch of Frodsham Bridge (**photo 06.8**), and the sandstone walls of the Town Quay (**photo 06.9**) downstream of the bridge, are visible but inaccessible.
- There are footpaths from the north and south sides of Bridge Lane to adjacent townscape areas and the rural footpath network. Footpath 70 to Langdale Way has been re-routed and improved in conjunction with the Bridgewater Close development (see **photo 07.3**).



Planning Guidance

- 1. All new development that can be seen from the Sutton Causeway / A56 approach, and on the river frontage, should respect and enhance the character of the eastern gateway into Frodsham and the listed Frodsham Bridge (photo 06.8; 0.14).  
(LPP: BE1; BE5; BE6; BE8)
- 2. Ensure signage adjacent to Frodsham Bridge (LB) and associated with Weaver Park Industrial Estate avoids adding visual clutter (photo 06.10).  
(LPP: BE16)
- 3. All new development should protect and enhance the listed 22-arch Frodsham Viaduct no.53 Chester – Warrington Railway (photo 05.4; 06.9).  
(LPP: BE1; BE5; BE6; BE8)
- 4. All new development that includes extensions and alterations to existing dwellings should maintain the character of the original building, harmonise with the existing building and have respect for neighbouring properties, e.g. Barley Groves (photo 06.11).  
(LPP: BE1; H4; H8)
- 5. All new development should maintain and repair existing sandstone walls, copings and gate piers using traditional methods and materials (photo 06.12).  
(LPP: BE1)
- 6. All new development should respect and enhance the setting of the cast iron milepost at the west end of Bridge Lane (photo 06.13).  
(LPP: BE1)
- 7. All new development should protect and improve existing community green space in the Quay Side development (photo 06.14).  
(LPP: NE8; BE1)
- 8. All new development should protect and interpret the roadside geological exposure to the east of the former United Reform Church (photo 0.03).  
(LPP: NE1; NE7; BE1)
- 9. All new development should ensure that views of the River Weaver and railway viaduct from Frodsham Bridge are maintained and enhanced (photo 06.16; 06.9).  
(LPP: BE1)

- 10. All new development should respect and interpret the heritage assets of the Frodsham Bridge area, including the Town Quay, sites of former riverside industry and the medieval salt route (PROW72) (photo 06.15).  
(LPP: NE7; NE8; BE1; BE5; BE6; BE8)
- 11. Maintain safe public rights of way for shared use by horse riders, pedestrians and cyclists.  
(LPP: T8; T9)

Community Aspirations

- Local people desire interpretation of heritage assets in the Frodsham Bridge area to convey the legacy of Frodsham’s river port and Newtown settlement.  
(LPP: NE7; NE8; BE1; BE8)
- Local people desire the area’s urban footpaths to be maintained and promoted as traffic free routes.  
(LPP: T8)
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.





06.1 Petrol Station and Business Centre dominate the S side of Bridge Lane.



06.2 Fraser House, Bridge Lane, in business use.



06.3 Town houses and apartments in Bridgewater Close.



06.4 Residential Manor Farmhouse complex.



06.5 East Bank and East Bank Cottage (LIBs).



06.6 Former United Reform Church (LIB) and outcrop of Tarporley Siltstones to right..



06.7 The Quay (part of Route 5 NCN).



06.8 Derelict path under Frodsham Bridge, western arch, and terraced cottages on Bridge Lane.



06.9 Town Quay (left); railway viaduct; and water-side frontage of Weaver Park Industrial Estate.



06.11 Barley Groves 1879, Bridge Lane.



06.10 Eastern approach to Frodsham Bridge (LB).



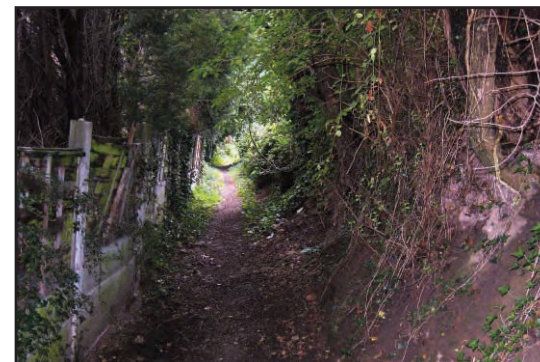
06.12 Sandstone Walls fronting Edwardian property, Bridge Lane.



06.13 Milepost, Bridge Lane.



06.14 Quay Side green space.



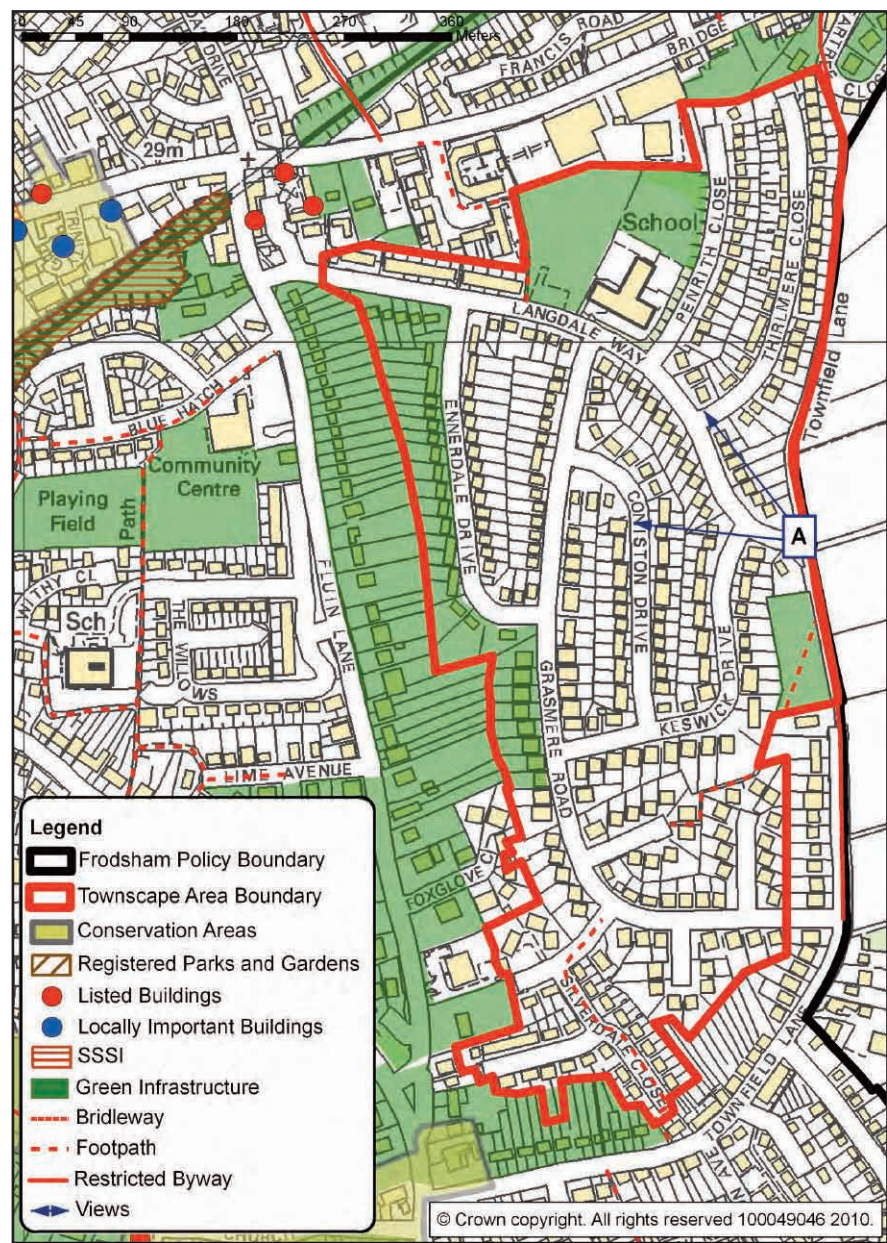
06.15 PROW72 – a well worn hollow way.



06.16 Upstream view of the River Weaver from Frodsham Bridge.



Townscape Area 07: The Lakes



Key characteristics

- This area is popularly called The Lakes as the roads are named after places in the Lake District.
- There are two access roads to The Lakes: off Fluin Lane and from Townfield Lane making Langdale Way and Grasmere Road the main thoroughfare. Many of the side roads are cul-de-sacs.
- The area is on the eastern side of town on land that rises to the south and east. The medieval salt route / hollow way, leading to Bridge Lane and the former river port, marks the area's eastern edge and the Town Policy Boundary.

- The Lakes is a quiet and leafy residential area designed with open-plan front gardens that was mostly developed in the 1960s.
- There is a panoramic view from the eastern end of Langdale Way to the mouth of the River Weaver, the chemical works in Runcorn, and across the Mersey Estuary towards Liverpool (view A). Beacon Hill and Overton Hill are clearly visible to the southwest from several locations in the area.

Townscape character description

- The Lakes includes a primary school and a small group of commercial premises in Langdale Way (photo 07.1), and a children's playground on the eastern edge of the area.
- Properties vary in size and type: they are mostly 2-storey detached houses and dormer bungalows, e.g. Penrith Close and Borrowdale Close, with semi-detached houses in Silverdale Close and Thirlmere Close, and semi-detached bungalows in Keswick Drive.
- Building materials are predominantly brick with tiled roofs in a variety of modern styles
- Throughout the area there is evidence of many loft conversions and over-garage extensions (photo 07.2).
- Many properties have garages and off-road car parking.
- Most front gardens are open plan and mature. Rear gardens on the western edge of The Lakes contribute to green infrastructure and the biodiversity corridor shown on Map 2.2.
- There are footpaths from Langdale Way (photo 07.3) to Bridge Lane and from Borrowdale Close to the Playground on Townfield Lane.

Planning Guidance

1. All new development that includes extensions and alterations to existing property should maintain the character of the original building, harmonise with the existing building and have respect for neighbouring properties.  
(LPP: BE1; H4; H8)
2. New development should protect the continuity of the biodiversity corridor to the rear of Grasmere Road and Ennerdale Drive.  
(LPP: BE1)
3. All new development should protect and enhance Townfield Lane Playground (photo 07.4) (LPP: BE1; RT4)



4. Development should maintain and enhance the character of the hollow way extension to Townfield Lane (PROW72) east of Thirlmere Close (photo 06.15).  
(LPP: T8)
5. All new development should ensure views to distant landscape features are protected, e.g. the panoramic view from the east end of Langdale Way to the Mersey Estuary (photo 07.5).  
(LPP: BE1)
6. Maintain safe public rights of way for shared use by horse riders, pedestrians and cyclists.  
(LPP: T8; T9)

### Community Aspirations

- Local people desire the estate to retain its open-plan front gardens (photo 07.6).  
(LPP: BE1)
- Local people desire that the area's urban footpaths be maintained and promoted as traffic free routes (photo 07.3).  
(LPP: T8)
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.



07.1 Businesses and shops in Langdale Way.



07.2 Loft conversions and over-garage extensions in Grasmere Road.



07.3 Footpath 70 from Langdale Way to Bridge Lane.



07.4 Townfield Lane Playground.



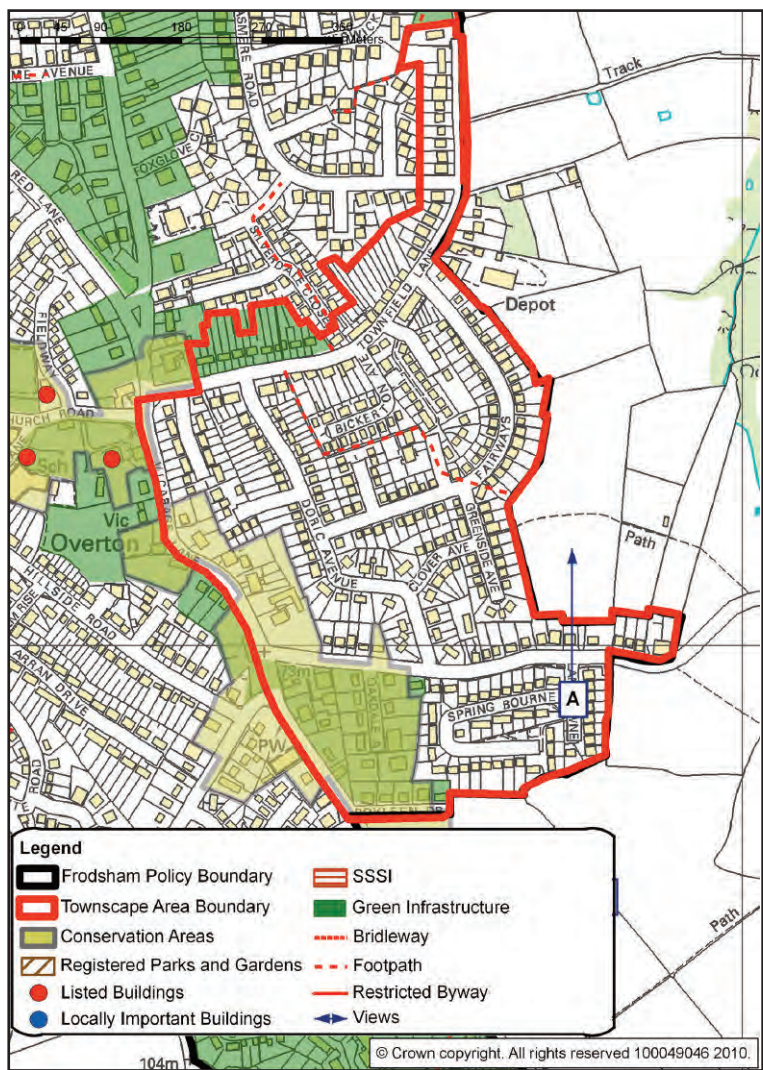
07.5 View NW to Halton and Merseyside from the E end of Langdale Way.



07.6 Open plan front gardens, Grasmere Road.



Townscape Area 08: Doric Avenue



Key characteristics

- Doric Avenue is central to this townscape area and links two busy access routes: Townfield Lane and Bradley Lane.
- This is primarily a residential area on the eastern edge of the town on land that rises to the south and east.
- The western part of the Doric Avenue Townscape Area is in the Frodsham AAP and Frodsham (Overton, Five Crosses) Conservation Area. This part includes 3 locally important buildings and 7 unlisted buildings of ‘townscape merit’ (Appendix 3).
- There is little off-road parking provision in parts of Townfield Lane which carries industrial traffic to and from a haulage depot just outside the Town Policy Boundary. Similarly, Bradley Lane carries industrial traffic to Bradley Orchard Farm and considerable private and commercial traffic to the leisure facility at Catton Hall.

- There is a distant view to Halton Hill and Fiddlers Ferry Power Station from Lansdowne (view A). The War Memorial and Beacon Hill can be seen from several locations in the area.

Townscape character description

- The Doric Avenue Townscape Area is predominantly residential and reflects development that has taken place since pre-Victorian times to the late 20th century resulting in a wide variety of property types and styles.
- The western part of the area in and adjacent to the conservation area mainly comprises older properties in a variety of sizes and individual styles that display Georgian, Victorian and Edwardian architectural features, e.g. West View (LIB); High Lea (LIB); The Beeches + The Elms: they make a major contribution to townscape character.
- Decorative brickwork detail on older properties in the conservation area, e.g. Birch Terrace and Overton Hall (LIB), adds further interest to townscape character.
- The former Victorian Sunday School in Bradley Lane (TM) has been sensitively converted into a dwelling (photo 08.1) as have the outbuildings at Overton Hall (photo 08.3).
- In the north, the central section of Townfield Lane is characterised by older property with small front gardens enclosed mainly by sandstone or brick walls. Many of the individual dwellings show extensions and alterations that differ from the original style (photo 08.6).
- Inter-war and early post-war houses, many of them semi-detached and partly pebble-dashed, occupy the eastern end of Townfield Lane and parts of Doric Avenue. There are properties of a similar age and fabric in Bradley Lane and Townfield Lane between Fairways and Grasmere Road.
- Pollard Avenue is a small development of refurbished and rendered “prefabs” in open plan gardens (photo 08.2) with a communal parking area.
- Bungalows characterise Bickerton Avenue and the northern end of Townfield Lane at the Green Belt / Town Policy Boundary.
- The central and eastern part of the Doric Avenue area comprises detached houses developed during the 1960s and 1970s: the older Fairways estate has been changed by a range of building alterations though most properties have retained their original low, stone-coloured garden walls. In contrast, the 1970s Springbourne estate is open-plan with a mixture of detached houses, and dormer bungalows in Lansdowne.



- The paddock adjacent to Royleen Drive, but in the Green Belt, is part of the conservation area and an important link with open countryside.
- Throughout the Doric Avenue area gardens are mature and some are large. Property boundaries are diverse and include a variety of walls, fences and hedges (photo 08.8).
- Footpaths connect Townfield Lane to Silverdale Close in the north and Rosewood Avenue in the south, and Greenside Avenue to open countryside. There is also pedestrian access from Bradley Lane to the western part of Springbourne.

Planning Guidance

1. All new development should safeguard and improve the quality of the environment within and adjacent to the Conservation Areas (photos 08.1; 08.3). (LPP: BE1; BE8; BE10; BE11; BE19; BE22; BE23)
2. All new development should resist further demolition and replacement of dwellings to retain the diverse age range and character of the area. (LPP: BE1; H4; H10)
3. All new development should resist further plot division and infill to retain the individual distinctiveness and biodiversity of the area. (LPP: NE7; BE1; H4; H5)
4. All new development that includes subdivision of existing dwellings into self-contained residential units should ensure that the character of the original building is maintained or enhanced. (LPP: BE1; H4; H7)
5. All new development that includes extensions and alterations to existing property should maintain the character of the original building, harmonise with the existing building and have respect for neighbouring properties. (LPP: BE1; H4; H8)
6. All new development should protect and manage existing trees, hedges and green spaces within the area, e.g. in Oakdale Avenue in the conservation area and Springbourne (photo 08.4) to augment green infrastructure and the biodiversity corridors as shown on Map 2.2. (LPP: NE7; NE8; NE9; BE1)
7. Development should retain and manage grass verges and green spaces, e.g. on Bradley Lane and Doric Avenue, to maintain townscape character (photo 08.5). (LPP: BE1)

8. All new development should maintain and repair existing sandstone walls, copings and gate piers using traditional methods and materials (photo 08.6). (LPP: BE1)
9. Appropriate redevelopment of the haulage depot, accessed from Townfield Lane but in the Green Belt, should be encouraged if it reduces the flow of heavy goods vehicles through this residential area. (LPP: GS3)
10. All new development should protect the view from Lansdowne to Halton Hill and Fiddlers Ferry Power Station. (photo 08.7) (LPP: BE1)

Community Aspirations

- The community hopes that front garden boundaries will be maintained to protect the visual continuity of the street scene (photo 08.8). (LPP: NE7; BE1)
- Local people desire that the area’s urban footpaths be maintained and promoted as traffic free routes. (LPP: T8)
- The community hopes that properties on the list of locally important buildings, drawn up by VRBC in 1977-79, and other properties – some described as ‘unlisted buildings of townscape merit’ and identified in the Frodsham (Overton, Five Crosses) Conservation Area Appraisal – will be assessed and/or re-evaluated for inclusion on a revised 21st century Local List.
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.





08.1 Former Sunday School, Bradley Lane TM, now a private dwelling.



08.2 Refurbished "prefabs", Pollard Avenue.



08.3 Overton Hall, Vicarage Lane LIB, with outbuildings converted to residential units.



08.4 Green space in Springbourne.



08.5 Grass verges, Bradley Lane.



08.6 Sandstone walls and gate piers, Townfield Lane.

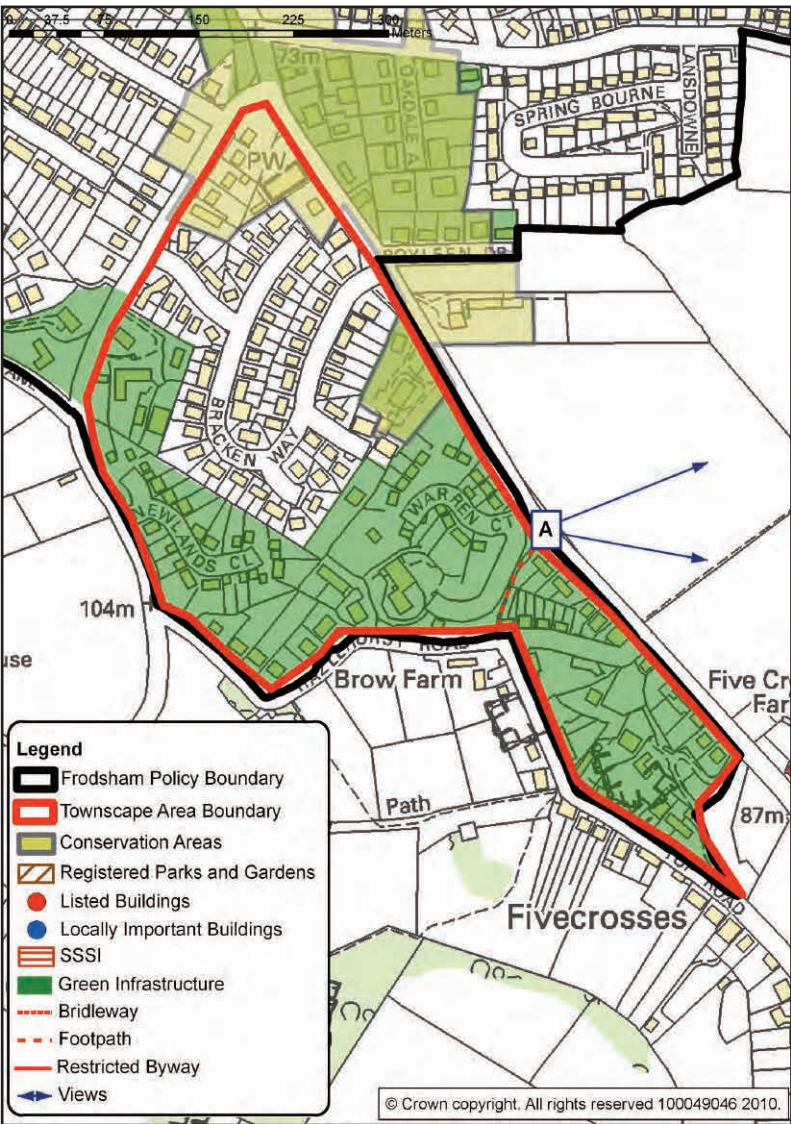


08.7 View N to Halton Hill from Lansdowne.



08.8 Well-managed hedges, Townfield Lane.

Townscape Area 09: Five Crosses



Key characteristics

- Five Crosses is the popular name for this part of Frodsham which extends southwards from Fivelanes End: the junction between Kingsley Road, Manley Road and 3 others.
- The area lies on the southern edge of town, between Kingsley Road and Manley Road, on land that rises to the southwest below the wooded slopes of Beacon Hill.
- Part of the area lies within the Frodsham (Overton, Five Crosses) Conservation Area. The area includes 3 locally important buildings (LIB) and 2 unlisted buildings of 'townscape merit' (TM) (Appendix 3).
- Kingsley Road (B5152) is the main route into Frodsham from the south and Manley Road is busy with local traffic, e.g. to and from Forest Hills Hotel and leisure complex, Frodsham Golf Club and Hillcrest Residential Care Home. The remainder of the area is quiet.



- There is a panoramic view across the Weaver Valley towards the South Pennines from the eastern end of Warren Court (view A). The wooded slopes of Beacon Hill form a backcloth to views west from Kingsley Road.

**Townscape character description**

- Five Crosses Townscape Area is almost entirely residential, but includes former sandstone quarries, now wildlife habitat, on the west side of Kingsley Road (photo 09.1).
- The area reflects development that has taken place since pre-Victorian times to the late 20th century resulting in a wide variety of property types and styles, e.g. large Victorian houses such as Ingledene (TM), Kingsley Road, small terraced cottages on Top Road and modern detached and semi-detached properties on the Bracken Way estate. Most properties are brick built with slate or tile roofs.
- Three large houses formerly with extensive grounds have been modified in different ways over several decades: Eversley House (LIB) has been extended and converted into apartments and is surrounded by individually designed houses and a small 1980s open plan development, Warren Court (photo 09.2). Ingledene has been divided into 2 dwellings with an infill bungalow, built 2003, in the grounds (photo 09.6). Hillcrest has been extended and converted into a residential care home with several recently built detached properties in close proximity.
- Kingsley Road demonstrates mixed-age ribbon development and infill and many houses have sandstone garden walls. Properties in the south face open countryside beyond the Town Policy Boundary.
- A substantial sandstone house, adjacent to the closed Top Road Quarry (photo 09.3), has been extended in keeping with the original building and nearby cottages have been sensitively modernised.
- Frodsham Methodist Church (TM) is a distinctive yellow brick building that was recently extended and embellished with stained glass windows in a modern design (photo 09.4).
- The Bracken Way estate is a compact 1960s development with enclosed gardens and vehicular access from Kingsley Road and Manley Road. Newlands Close is a small 1970s open plan development.

- Special architectural features of buildings include: distinctive decorative brickwork, e.g. on Ingledene (TM); use of contrasting brick colours and textures; decorative render panels; timber framed gables and sandstone window and door surrounds, e.g. Eversley House (LIB) (photo 09.5).
- A footpath with steps links Kingsley Road to Hazlehurst Road near its junction with Top Road, and there is pedestrian access between Kingsley Road and the closed end of Bracken Way.

**Planning Guidance**

1. All new development should resist further plot division and infill to retain the individual distinctiveness and biodiversity of the area.  
(LPP: NE7; BE1; H4; H5)
2. All new development should resist demolition and replacement of Victorian and Edwardian properties to retain the diverse character of the area.  
(LPP: BE1; H4; H10)
3. All new development should respect and complement existing buildings and their fabric but this should not prejudice high quality and contextually-designed contemporary development. e.g. The Court at Ingledene (photo 09.6).  
(LPP: BE1; H4; H8)
4. All new development that includes extensions and alterations to existing property should maintain the character of the original building, harmonise with the existing building and have respect for neighbouring properties.  
(LPP: BE1; H4; H8)
5. All new development that includes subdivision of existing dwellings into self-contained residential units should ensure that the character of the original building is maintained or enhanced (photo 9.5).  
(LPP: BE1; H4; H7)
6. All new development should maintain and repair existing sandstone walls, copings and gate piers using traditional methods and materials (photo 09.7).  
(LPP: BE1)
7. All new development should promote the area's green infrastructure and biodiversity and its links to open countryside (photo 09.8).  
(LPP: NE7; BE1)
8. All new development should protect and manage mature trees, hedges and green spaces wherever possible, e.g. on Kingsley Road and Manley Road (photo 09.9), to augment green infrastructure and the biodiversity corridors shown on Map 2.2  
(LPP: NE7; NE8; BE1)



9. All new development should protect the view from Warren Court / Kingsley Road across the Weaver Valley to the South Pennines (photo 09.10).

(LPP: BE1)

## Community Aspirations

- Local people desire the area's urban footpaths to be maintained and promoted as traffic free routes.  
(LPP: T8)
- The community hopes that properties on the list of locally important buildings, drawn up by VRBC in 1977-79, and other properties – some described as 'unlisted buildings of townscape merit' and identified in the Frodsham (Overton, Five Crosses) Conservation Area Appraisal – will be assessed and/or re-evaluated for inclusion on a revised 21st century Local List.
- The community wishes to see the 30mph speed restriction zone begin near the Kingsley Road / Watery Lane junction to ensure greater safety for pedestrians in the southern part of Frodsham.
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.



09.1 Former quarry near Eversley House.



09.2 Warren Court, 1980s



09.3 Extended sandstone house, Top Road.



09.4 Frodsham Methodist Church TM, Kingsley



09.5 Eversley House (LIB), converted to apartments.



09.6 The Court – fabric and features complement neighbouring property.



09.7 Sandstone walls and gate piers, Kingsley Road.



09.8 Kingsley Road – part of the green infrastructure.



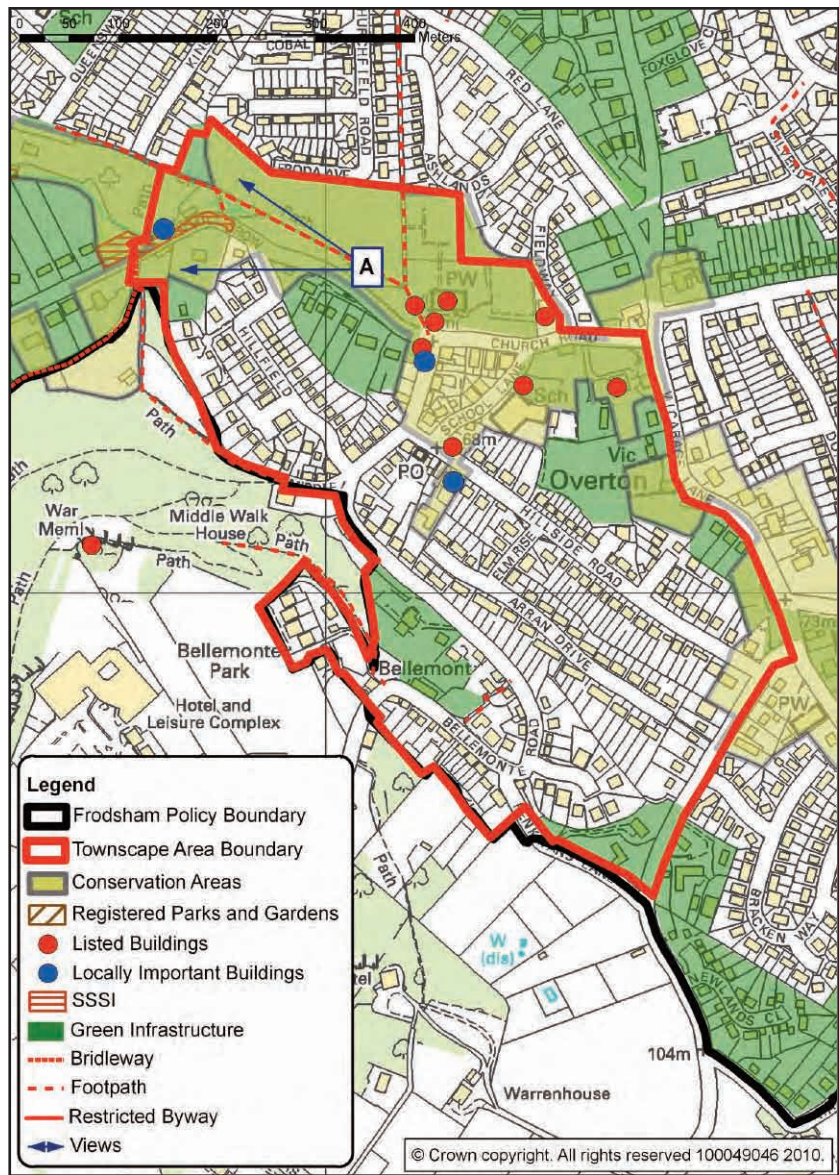
09.9 Mature trees and grass verges, Manley Road.



09.10 View E across the Weaver Valley from Warren Court.



Townscape Area 10: Overton



Key characteristics

- Overton Townscape Area has developed from the medieval settlement that grew up around the Parish Church. Today it is a diverse, mixed-age mainly residential area.
- The area to the south and east of the church is situated on a gently sloping shelf that rises to the Green Belt / Town Policy Boundary in the southwest.
- A large part of Overton lies in the Frodsham Area of Archaeological Potential and Frodsham (Overton, St Lawrence's) Conservation Area. A small part in the east lies within Frodsham (Overton Five Crosses) Conservation Area. There are 8 listed buildings, 7 locally important buildings and 9 unlisted buildings of 'townscape merit' (Appendix 3).

- Several roads are busy at peak periods especially near the school and local businesses, and on-street parking can inhibit free traffic flow. Vicarage Lane is part of the main route into Frodsham from the south (B5152).
- The rock exposures on Howey Lane are part of the Frodsham Railway and Road Cuttings Site of Special Scientific Interest (SSSI).
- A belt of interconnecting green spaces crosses the area from Churchfields through the Cemetery, adjacent private gardens and the school playing field to Overton Memorial Hall Bowling Green, forming an important component of Frodsham's green infrastructure.
- There is a superb uninterrupted view from Churchfields across the marshes and the Mersey Estuary (view A). The wooded upper slopes of Overton Hill and Beacon Hill can be seen from several locations in the area.

Townscape character description

- Overton Townscape Area is mainly residential but includes a core of small businesses and community facilities centred on School Lane and the northern ends of Hillside Road and Bellemonte Road.
- The twelfth century, Grade I listed sandstone church (photo 10.1) was substantially restored in the late 19th century, the Church of England Primary School and nursery (LIB), a single story brick building, and 2 public houses (photo 10.1A), predominantly in white render, add character to the Overton core.
- Since World War 1 Overton has experienced extensive residential growth. Some development has involved replacement of old property, some infill e.g. on Bellemonte Road (photo 10.2), and some backfill e.g. at Hillview Close (photo 10.3).
- Among the oldest residential properties are: The Old Vicarage (LB), early 18th century and extended at the front in the late 19th century in Jacobean style, Church Farm (LB), a mid 17th century property in Church Road (photo 10.3A) and 56 Hillside Road (LB), an oak-frame and sandstone cottage dating from the early 17th century.
- At the eastern end of Hillside Road is a distinctive house in arts and crafts style (TM) and opposite is the former Five Crosses Infants School (LIB), now a newly extended scout hall. Hillside Road also includes post-war bungalows for the elderly (photo 10.4).
- 20th century dwellings include terraced properties in Bellemonte Road, individually designed detached houses in Hillfield and Pin Mill Brow, many semi-detached or detached houses, e.g. Elm Rise, Arran Drive and Greenacres built in the 1970s, and detached bungalows of various ages and styles scattered throughout the area.



- Most dwellings are brick built and older properties have decorative brickwork detail and distinctive local design features. Several houses in School Lane and Church Road have replicated this detail on property extensions (photo 10.5).
- Most older gardens have perimeter fences, hedges or walls of brick or sandstone; walls of local sandstone are a distinctive feature on Bellemonte Road. In addition, ‘Cheshire railings’ more commonly found as field boundaries, can be seen at the junction of School Lane with Church Road (photo 10.6). In contrast, the front gardens to recent infill development are often open-plan.
- The green infrastructure that extends from Overton Memorial Hall (TM) bowling green to Churchfields forms part of the biodiversity corridor that connects to Overton Hill in the vicinity of Bottom Walk as shown on Map 2.2.
- Footpaths in this area offer a pedestrian route to the commercial core (photo 10.7) and link to the network of paths on Overton Hill and The Sandstone Trail.

Planning Guidance

1. All new development should safeguard and improve the quality of the environment within and adjacent to the Conservation Areas (photos 10.1; 10.1A; 10.3A; 10.6; 10.7; 10.8).  
(LPP: BE1; BE4; BE10; BE11; BE19; BE22; BE23)
2. All new development should resist further plot division and infill to retain local distinctiveness, especially on Church Road, Bellemonte Road and School Lane, and the biodiversity of the area (photos 10.6; 10.11; 10.3A).  
(LPP: NE9; BE1; BE5; BE6; BE7; BE8; BE10; BE11; BE22; BE23; H4; H5)
3. All new development should resist demolition and replacement of pre-20th century property to retain the diverse character of Overton.  
(LPP: BE1; H4; H10)
4. All new development that includes subdivision of existing dwellings into self-contained residential units should ensure that the character of the original building is maintained or enhanced.  
(LPP: BE1; H4; H7)
5. All new development that includes extensions and alterations to existing property should maintain the character of the original building, harmonise with the existing building and have respect for neighbouring properties.  
(LPP: BE1; H4; H8)

6. All new development should maintain and repair existing sandstone walls, copings and gate piers using traditional methods and materials (photo 10.11).  
(LPP: BE1)
7. All new development should protect and manage mature trees, hedges and green spaces wherever possible, e.g. on Pinmill Brow, Church Road, Vicarage Lane and Manley Road, to augment green infrastructure and the biodiversity corridors shown on Map 2.2.  
(LPP: NE7; NE8; BE1)
8. All new development should protect and improve Churchfields as public open space (photo 10.9).  
(LPP: BE1; RT4)
9. All new development should protect and interpret the geological exposure on Pinmill Brow (photo 10.10).  
(LPP: NE2; NE7; BE1)
10. All new development should ensure that the view across the marshes and the Mersey Estuary from Churchfields is protected (photo 10.9).  
(LPP: BE1)

Community Aspirations

- Local people desire that the area’s footpaths be maintained and promoted as traffic free routes to the town centre and countryside (photo 10.12).  
(LPP: T8)
- The community hopes that properties on the list of locally important buildings, drawn up by VRBC in 1977-79, and other properties – some described as ‘unlisted buildings of townscape merit’ and identified in the Frodsham (Overton, St Lawrence’s) and Frodsham (Overton Five Crosses) Conservation Area Appraisals – will be assessed and/or re-evaluated for inclusion on a revised 21st century Local List.
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.





10.1 Parish Church of Saint Laurence (LB)



10.1A Ring o'Bells Inn, Belmonte Road



10.7 Footpath 60, Church Entry, from Church Road to Red Lane.



10.8 Overton Memorial Hall (LIB) and Bowling Green.



10.2 Infill with retained sandstone wall on Bellemonte Road



10.3 Hillview Close, backfill off Hillside Road.



10.9 Churchfields with views to the Mersey Estuary.



10.10 Frodsham Beds below laminated Tarporley Siltstones (SSSI), Pinmill Brow.



10.3A Church Farm (LB) in the conservation area



10.4 Housing for the elderly with SUDS parking area, Hillside Road.



10.11 Sandstone walls are a feature on School Lane.



10.12 Footpath 22, a steep shortcut on Bellemonte Road.



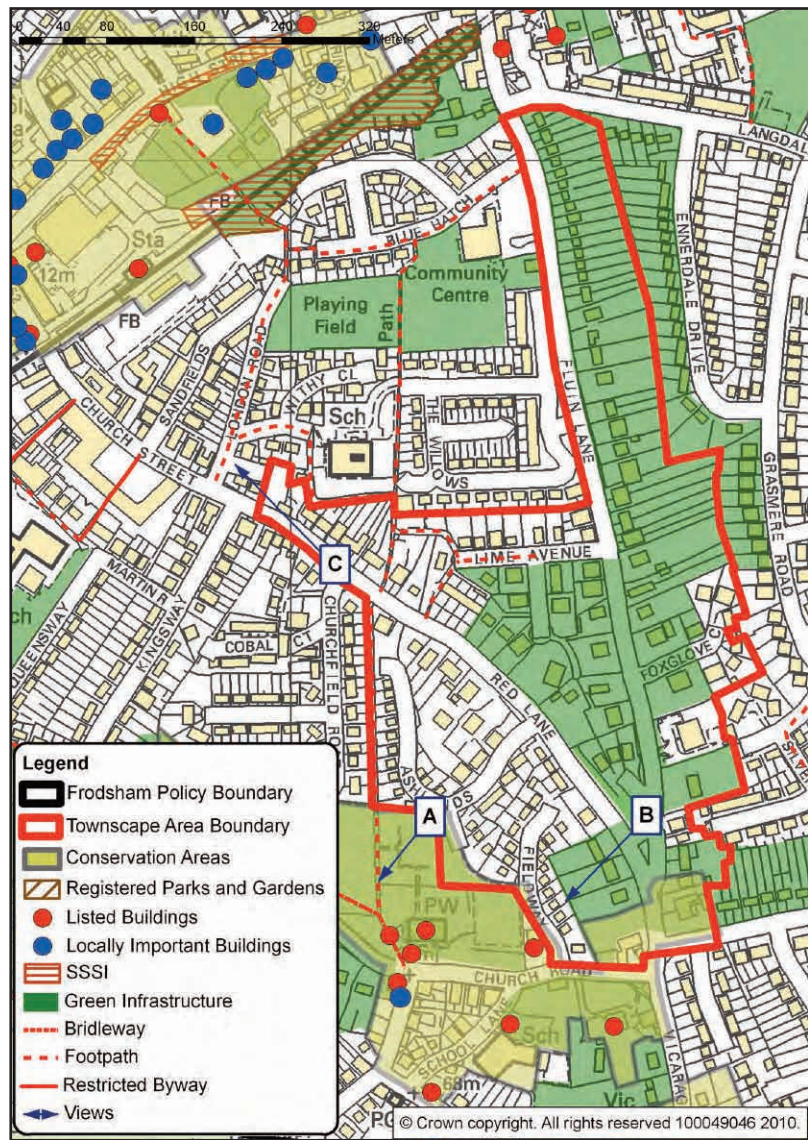
10.5 Sensitive extension of a School Lane property in the conservation area.



10.6 Cheshire railings enclosing a garden in the conservation area.



Townscape Area 11: Fluin Lane



Key characteristics

- Fluin Lane links the main route from the south (B5152) with the A56 avoiding the town centre.
- The Fluin Lane Townscape Area extends southwards from the western end of Langdale Way and rises steadily to Townfield Lane. The area's other key road is Red Lane which runs downslope into the southern half of Church Street.
- This is a diverse mixed-age residential area in which green infrastructure is prominent. The southern tip is part of Frodsham (Overton, St Lawrence's) Conservation Area and includes 1 locally important building and 2 unlisted buildings of townscape merit (Appendix 3).
- The two main roads are very busy with traffic, whilst the many cul-de-sacs are much quieter.

- There are good views from the top of Ashlands and Fluin Lane to Overton Hill in the southwest (views A & B) and on a clear day Hale Lighthouse, on the north shore of the River Mersey, can be seen from Red Lane (view C).

Townscape character description

- The Fluin Lane Townscape Area is entirely residential and development ranges from Victorian to 21st century in age.
- Properties in the conservation area include the whitewashed brick cottage, Brockton Mews (LIB), its large, ornate, yellow brick neighbour, Brockton House (TM) (photo 11.1A) and a pair of large semi-detached houses (TM) in yellow brick with decorative brick banding in a contrasting colour. A 21st century house has been built in the garden of one of them.
- Property on the eastern side of Church Street includes mainly elevated terraced and semi-detached houses with sandstone front walls and gate piers and vehicular access from Back Lane to the rear (photos 11.9; 11.1).
- There is a row of houses, built between the wars, at the northern end of Fluin Lane and older, mainly individually designed, houses towards the southern end.
- Several properties in Beech Avenue and the southern end of Fluin Lane display local design details including decorative brickwork, render panels, stone sills and lintels, and timber framed gables, which give the area a special character (photo 11.2).
- Ashlands and Fieldway are small 1970s estates of mainly detached houses in neo-Georgian style linked by a footpath through well-managed green space (photo 11.3). Larger and newer detached houses in Ashlands have been built in the garden of an older Red Lane property.
- Towards the southern end of Fluin Lane, on the east side, is Foxglove Court, a 1990s in-garden and backfill development of 6 detached houses and a bungalow, and Delamere House, a 21st century block of 16 apartments built on the plot of a demolished property. Both developments have used modern equivalents of traditional building materials and incorporated local design features from nearby older properties (photo 11.4).
- The area's green infrastructure comprises public green spaces, large gardens and mature trees, especially within the Fluin Lane, Red Lane and Beech Avenue triangle (photo 11.5). This verdant character continues into the long rear gardens of Fluin Lane properties to form an important component of Frodsham's biodiversity corridor shown on Map 2.2.
- Several urban footpaths provide short-cuts to main roads and adjacent townscape areas.



Planning Guidance

- 1. All new development should resist further plot division and infill to retain individual distinctiveness and biodiversity especially within the Fluin Lane, Red Lane and Beech Avenue triangle.  
(LPP: NE7; BE1; H4; H5)
- 2. All new development should resist demolition and replacement of Victorian and Edwardian properties to retain the diverse character of the area.  
(LPP: BE1; H4; H10)
- 3. All new development that includes subdivision of existing dwellings into self-contained residential units should ensure that the character of the original building is maintained or enhanced.  
(LPP: BE1; H4; H7)
- 4. All new development that includes extensions and alterations to existing property should maintain the character of the original building, harmonise with the existing building and have respect for neighbouring properties.  
(LPP: BE1; H4; H8)
- 5. All new development should maintain and repair existing sandstone walls, copings and gate piers using traditional methods and materials (photo 11.6).  
(LPP: BE1)
- 6. All new development should protect and manage existing trees, hedges and green spaces (photo 11.3, 11.5) and especially the continuity of adjoining gardens to the rear of Fluin Lane to augment the town's green infrastructure and the biodiversity corridors shown on Map 2.2.  
(LPP: NE7; NE8; BE1)
- 7. All new development should ensure views from the top of Fluin Lane and Ashlands to Overton Hill and the War Memorial and from Red Lane to Hale Lighthouse are protected (photo 11.7).  
(LPP: BE1)

Community Aspirations

- Local people desire that the quality and character of the area's urban footpaths be maintained and promoted as traffic free routes.  
(photo 11.8)  
(LPP: T8)
- The community hopes that properties on the list of locally important buildings, drawn up by VRBC in 1977-79, and other properties – some described as 'unlisted buildings of townscape merit' and identified in the Frodsham (Overton, St Lawrence's) Conservation Area Appraisal – will be assessed and/or re-evaluated for inclusion on a revised 21st century Local List.
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.



11.1 Back Lane service road for Church Street E.



11.1A Brockton House (TM) Fluin Lane



11.2 Edwardian property, Fluin Lane.



11.3 Green space and footpath linking Fieldway and Ashlands.





11.4 Delamere House, 2006, Fluin Lane, with features reflecting those on older property.



11.5 Junction of Red Lane (left) and Fluin Lane.



11.6 Sandstone walls, Holly Bank, Church Street.



11.7 View SW to Overton Hill from Ashlands.

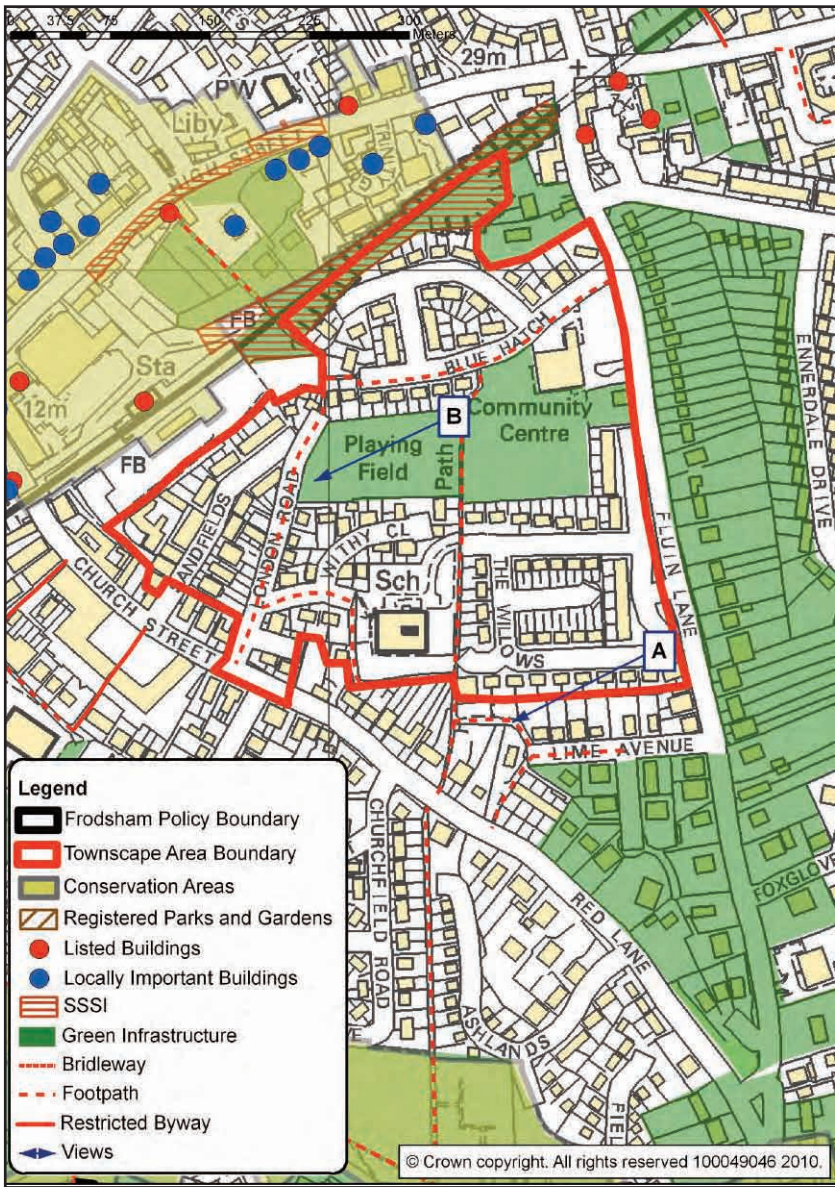


11.8 Footpath 61 linking Red Lane to the Community Centre area.



11.9 Church Street Property.

Townscape Area 12: Community Centre



Key characteristics

- This area takes its name from the busy Community Centre complex: its building, large car park and 2 fields have vehicular access from Fluin Lane and can be reached on footpaths from the north, south and west.
- The Community Centre Townscape Area lies between Church Street and Fluin Lane with railway property forming its northern boundary. The land rises gently to the southeast.
- The area is primarily residential and St Luke's Catholic Primary School is the only other facility.



- Properties close to the railway in Blue Hatch are subject to intermittent train noise, whereas, those close to Fluin Lane and Church Street experience road traffic noise; in contrast, the cul-de sacs leading from the 2 key roads are much quieter.
- On-road parking in Sandfields and London Road leads to congestion especially at the start and close of the school day.
- There are views from The Willows and Community Centre fields to the Mersey Estuary and the Clwydian Range in northeast Wales (views A & B) and Beacon Hill and Overton Hills can be seen from several locations in the area.

**Townscape character description**

- A large amount of 1960s development surrounds older property in this area. Some of the oldest dwellings include The Cottage and its sandstone walls on Fluin Lane and the slate-roofed Victorian / Edwardian terraced and semi-detached properties in Sandfields and nearby Derfel Terrace.
- Specific design features that contribute to local distinctiveness include decorative brickwork detail on eaves, gable ends and chimneys, terracotta roof ridge tiles, doorways and front wall detail in Sandfields (photo 12.1) and polychrome brickwork on Derfel Terrace (photo 12.2). A vacant former builder’s yard lies behind properties on the northwest side of Sandfields.
- Townscape character is dominated by the Community Centre and primary school plus 5 distinct housing developments, namely: The Willows – semi-detached houses and 3-storey terraced town-houses (photo 12.3); Blue Hatch – semi-detached houses and a core of bungalows; Whitehall Place – semi-detached houses (photo 12.4) ; Withy Close – semi-detached bungalows and Sandfields Court: 3-storey flat roofed town houses and 2 pairs of semi detached houses at the end of the cul-de-sac.
- The Willows town-houses facing Fluin Lane are set back behind a linear green space with trees (photo 12.5).
- Public green space / playing fields at Frodsham Community Centre and lawns around St Luke’s Catholic Primary School offer a green lung in the centre of a built up area where gardens are small.
- A network of urban footpaths provides access within the area, and to the train station and town centre

**Planning Guidance**

1. All new development should resist demolition and replacement of Victorian and Edwardian properties to retain the diverse character of the area.  
(LPP: BE1; H4; H10)
2. All new development that includes extensions and alterations to existing property should maintain the character of the original building, harmonise with the existing building and have respect for neighbouring property.  
(LPP: BE1; H4; H8)
3. All new development should maintain and repair existing sandstone walls, copings and gate piers using traditional methods and materials (photo 12.1; 12.5).  
(LPP: BE1)
4. All new development should protect and enhance existing open spaces adjacent to Frodsham Community Centre for public use (photo 12.6).  
(LPP: NE7; RT4)
5. All new development should protect and manage trees within the area especially the avenue between The Willows and Fluin Lane (photo 12.5), plus those on the Community Centre fields, to augment green infrastructure and the biodiversity corridor shown on Map 2.2.  
(LPP: NE7; BE1)
6. All new development should protect the views to the Mersey Estuary and the Clwydian Range from The Willows and Community Centre fields (photo 12.7).  
(LPP: BE1)

**Community Aspirations**

- Local people desire that the area’s urban footpaths be maintained and promoted as traffic free routes (photo 12.8).  
(LPP: T8)
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.





12.1 Decorative facade and front wall detail, Sandfields



12.2 Derfel Terrace.



12.3 The Willows townhouses.



12.4 Whitehall Place and Overton Hill.



12.5 Lime trees between Fluin Lane and The Willows townhouses (right).



12.6 Public open space, Frodsham Community Centre.

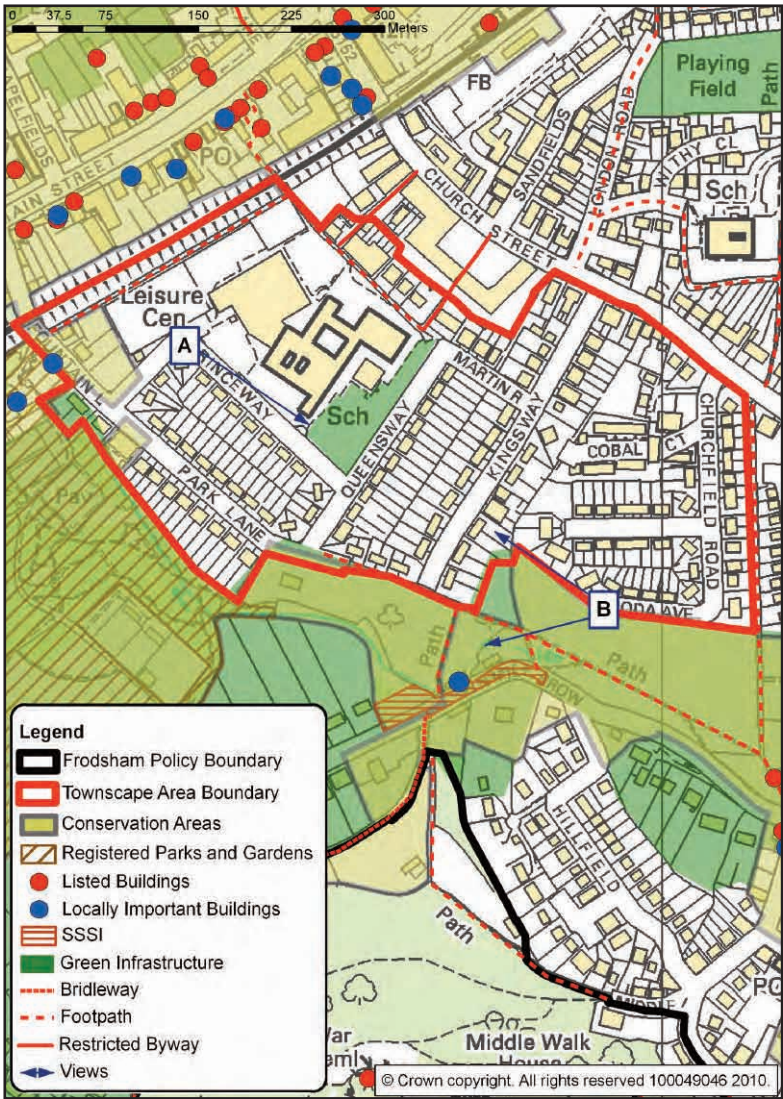


12.7 View WSW to Clwydian Range from Frodsham Community Centre.



12.8 Part of Footpath 61 from Blue Hatch to Red Lane.

Townscape Area 13: Kingsway



Key characteristics

- Kingsway is a long, tree-lined cul-de-sac running across the area with pedestrian access to Park Lane, also tree-lined. The land rises gently from the railway in a southerly direction and steepens markedly from the junction of Church Street and Churchfield Road.
- The Kingsway Townscape Area is bounded by the railway embankment to the northwest and by the extensive green wedge of Castle Park (TA14) and Churchfields (TA10) to the south. The area between Kingsway and the railway embankment includes the Leisure Centre and former site of Frodsham College (secondary school), now demolished.
- Most of the remaining area is residential and a small part of Fountain Lane and Park Lane is within Frodsham (Castle Park Locality) Conservation Area (**Map 3.3**).



- There is a good view of the Parish Church tower from the north end of Princeway (view A) and of the marshes and the Mersey Estuary from Froda Avenue, adjacent to Churchfields. The wooded slopes of Overton Hill and the War Memorial can be seen from several locations within the area.

**Townscape character description**

- The Kingsway Townscape Area is predominantly residential and properties demonstrate a diversity of styles with ages ranging from Victorian to 21st century. Most are medium sized semi-detached houses with a few bungalows and some larger properties interspersed.
- Castle Park Lodge and its neighbouring semi-detached houses (photo 13.1), are amongst the oldest properties in the locality.
- There are 3 small clusters of Victorian / Edwardian development, e.g. in Church Street / Kingsway they include large individually designed houses (photo 13.2), and in Alvanley Terrace (photo 13.5) and Glebe Terrace in Park Lane they have polychrome brick features.
- A number of different uniform developments, each with distinctive design features characterise the Kingsway area, e.g. porches in Princeway (photo 13.7); bay windows on the southwest side of Park Lane (photo 0.10); and multiple flat windows in Froda Avenue, top of Churchfield Road and St Lawrence Road.
- There is a small amount of modern infill / backfill, most notably at Cobal Court, off Churchfield Road, where a complex of two storey flats for older people was developed in the 1990s (photo 13.3); in addition, single houses have been built in the gardens of properties in Kingsway & Churchfield Road.
- Footpaths aid pedestrian movement both within the area and to adjacent areas (photo 13.4).

**Planning Guidance**

1. All new development should resist demolition and replacement of Victorian and Edwardian dwellings to retain the diverse character of the area.  
(LPP: BE1; H4; H10)
2. All new development that includes extensions and alterations to existing property should maintain the character of the original building, harmonise with the existing building and have respect for neighbouring properties.  
(LPP: BE1; H4; H8)

3. All new development that includes subdivision of existing dwellings into self-contained residential units should ensure that the character of the original building is maintained or enhanced.  
(LPP: BE1; H4; H7)
4. The development of the Frodsham College site for a health centre should safeguard and improve the quality of the adjacent residential environment (photo 13.12).  
(LPP: BE1; BE4)
5. The development of the Frodsham College site should protect and enhance the integral and peripheral green space (photo13.6; 13.8).  
(LPP: NE7; NE8; BE1; BE4)
6. The development of the Frodsham College site should improve pedestrian and vehicular access for both Leisure Centre and Health Centre users.  
(LPP: BE1; BE4; RT3; RT4; T8; T9)
7. All new development should maintain and repair existing sandstone walls, copings and gate piers using traditional methods and materials (photo 13.1; 13.2).  
(LPP: BE1)
8. All new development should protect the view from Princeway to the tower of St Laurence Church (photo 13.9) and from Froda Avenue to the marshes and the Mersey Estuary.  
(LPP: BE1)

**Community Aspirations**

- The community hopes that sustainable management of street trees will be implemented (photo 13.10).  
(LP: NE7; BE1)
- Local people desire that the character of the area’s urban footpaths be maintained and promoted as traffic free routes (photo 13.11).  
(LP: T8)
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.





13.1 Victorian property, Fountain Lane, within the Castle Park Conservation Area.



13.2 Edwardian property, Church Street.



13.9 View SE to the Parish Church tower from Princeway.



13.10 Street trees, Park Lane.



13.3 Cobal Court, off Churchfield Road.



13.4 Footpath 59 from Fountain Lane to Alvanley Terrace.



13.11 Footpath 56, with a distinctive drainage channel, NE side of Frodsham College site.



13.12 Frodsham Health Centre Development.



13.5 Alvanley Terrace.



13.6 Valuable green space bordering Queensway within the Frodsham College site



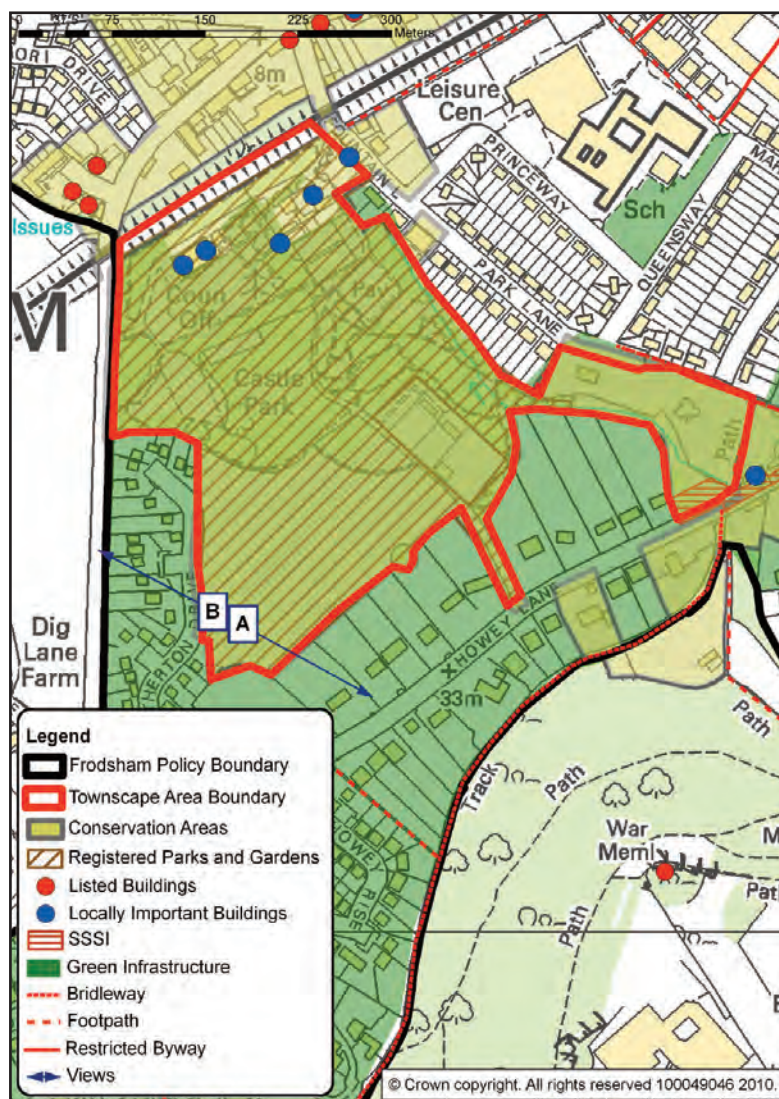
13.7 Properties with distinctive porches, Princeway.



13.8 Maturing trees, Frodsham College site.



## Townscape Area 14: Castle Park



### Key characteristics

- Castle Park was the site of the fortified manor-house / “castle” in late medieval times. Today it is a popular park on the western side of Frodsham, formerly in private ownership and gifted to the local authority in 1933 for the benefit of its inhabitants.
- Castle Park Townscape Area forms the major part of the Frodsham (Castle Park) Conservation Area; most of the park is a Registered Historic Park and Garden (GD 3508); the lower park is part of the Frodsham Area of Archaeological Potential and all of it is part of the green infrastructure (**Maps 2.2 and 3.3; Appendix 3**).
- Victorian buildings in the lower park, all refurbished or rebuilt, are sited on land close to the railway embankment that slopes down towards Fountain Lane in the northeast.



- Castle Park House, refurbished with a £2.2M Heritage Lottery Fund grant during 2005-06, overlooks level parkland to the south and a broad belt of mature deciduous trees on rising land that separates the lower park from the upper park beyond.
- More recently, Castle Park grounds have been refurbished with a £1.84m Heritage Lottery Fund Grant (2009-2010). The conservatory has now been restored, two new pavilions for park activities provided and two new play areas created.
- M56 noise and intermittent railway and over-flying aircraft noise can be intrusive in the lower park.
- The upper park is predominantly a sports field but includes the highest ground, capped by a gorse thicket, which affords fine views across the upper park to the War Memorial in the southeast (view A) and a distant view of the Mersey Estuary (photo 14.12) and mouth of the River Weaver to the northwest (view B).

### ***Townscape character description***

- The Victorian buildings provide a One-Stop-Shop for community affairs, office space for small businesses, the Castle Park Arts Centre (photo 14.1) and housing in Park Court.
- Walls of Cheshire brown brick with contrasting yellow/grey sandstone emphasise the classical design details of the House (photo 14.1A). Character is further enhanced with decorative brickwork detail especially on Park Court (photo 14.2), and retention of slate roofs and cast iron rainwater goods.
- Black, metal artwork gates to the Arts Centre courtyard and the bowling-green (photo 14.3) enhance the park with contemporary local craftsmanship.
- Pinmill Croft is a 20th century private residence set in an extensive orchard adjacent to the prominent valley and small stream which forms the eastern boundary of Castle Park.
- Castle Park displays the footprint of Edward Kemp's 1855 landscape design, including the conservatory and formal garden with dark clipped yews (photo 14.4), the American garden and the main car park, formerly the onamental pool.
- Other features of the modern park include: a new pedestrian entrance from Main Street/Chester Road (photo 14.5), open parkland with mature trees, and shady paths and sunlit glades in woodland areas.
- The attention to path detail and surface drainage in the American Garden is especially noteworthy (photo 14.6).
- The facilities for public recreation include bowls, tennis, athletics and a children's playground (photo 14.6A).



## ***Planning Guidance***

1. All new development should safeguard and improve the quality of the built environment within and adjacent to the conservation area.  
(LPP: BE1; BE8; BE10; BE11; BE15; BE19; BE21; BE22; BE23)
2. Protect and enhance the surviving design features of Edward Kemp's 1855 garden and pleasure grounds and maintain the provision of recreational facilities following the Heritage Lottery funded refurbishment, completed in 2010.  
(LPP: BE1; BE8; BE15; BE22; RT3)
3. All new development should protect and manage mature trees within the park wherever possible (photo 14.7).  
(LPP: NE9)
4. All new development should protect views within and out of the park, e.g. along Main Drive; through the brick arch into the formal garden and beyond (photo 14.8); from the area of the gorse thicket to the War Memorial and the Mersey Estuary.  
(LPP: BE1)
5. Development which materially affects the mound supporting the gorse thicket will not be allowed (*not included in the HLF project*) (photo 14.9).  
(LPP: BE1)
6. All new development should protect and manage the site of ancient Pinmill Orchard (photo 14.10; 14.12).  
(LPP: NE7; NE9)

## ***Community Aspirations***

- Local people hope that the distinctive valley and stream course to the east of Synagogue Well (*not included in the HLF project*) will be sensitively opened up to facilitate public access to geology (photo 14.11; 0.01).  
(LP: NE3; NE4; NE7)
- The community wishes to see Castle Park Main Entrance made safer by extending the 30mph speed restriction zone to the west of Netherton – see TA15 Community Aspirations.
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.





14.1 Castle Park Arts Centre (LIB).



14.1A Castle Park House (LIB)



14.2 Park Court (LIB), former farm building, now residential units, in the conservation area.



14.3 Gate to bowling green



14.4 Restored Conservatory (LIB) and Formal Garden.



14.5 New pedestrian entrance off Main Street.



14.6 Path detail in the American Garden.



14.6A Relocated playground.





14.7 Mature trees and parkland landscape.



14.8 View to re-instated fountains in the Formal Garden.



14.9 Former Flagpole Mount topped by a gorse thicket.



14.10 Ancient Pinmill Orchard within the conservation area.



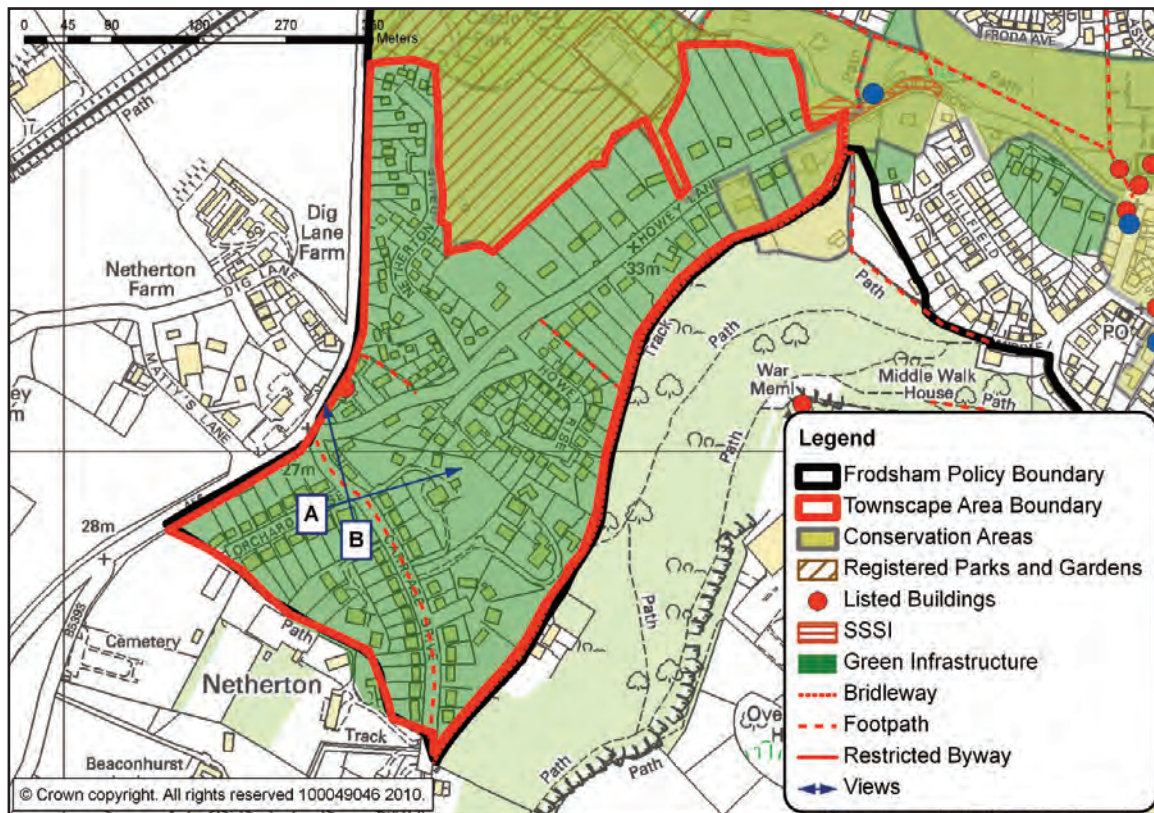
14.11 Stream course and sandstone outcrops E of Synagogue Well.



14.12 View B from Upper Park to Mersey Estuary.



## Townscape Area 15: Howey Lane



### Key characteristics

- Howey Lane was the route to the medieval settlement at Overton and is now a narrow residential lane providing access to the southern part of town from Chester Road (A56).
- The Howey Lane Townscape Area is characterised by its verdant appearance and is a valuable component of the town's green infrastructure. The area extends out to the Green Belt / Town Policy Boundary on 3 sides, and to Castle Park in the north.
- The eastern part of Howey Lane is part of the Frodsham (Overton, St Lawrence's) Conservation Area. There is 1 listed building, 1 locally important building and 2 unlisted buildings of townscape merit (**Appendix 3**).
- The north-western part of the area experiences intermittent railway noise and intrusive, continuous traffic noise from Chester Road (A56) and the M56.
- There is a good view to the War Memorial from Orchard Close (view A) and to the marshes and the Mersey Estuary from Carriage Drive (View B). The wooded slopes of Overton Hill provide a prominent backcloth to this area.



## ***Townscape character description***

- The Howey Lane area is totally residential with a predominance of extensive mature gardens (photo 15.1).
- Most properties, apart from those on Netherton Drive, are individually designed, detached brick dwellings. The majority were built after World War II and surround a small number of older properties.
- Netherton Drive, between Chester Road and Castle Park, is a sinuous cul-de-sac of houses in neo-Georgian styles, many of them sensitively extended.
- Rock Cottage and Rock Farm (LB), built largely of local sandstone (photo 15.2), front Chester Road.
- Carriage Drive has predominantly post World War II detached houses many of them with extensions; at its southern end there are 2 small cul-de-sacs of more recent development behind Carriage Drive properties.
- Erindale (LIB) has been converted into 3 residential units whilst retaining its original character (photo 15.3); nearby is Erin Lodge, Erin Cottage and 3 modern properties in contrasting styles all reached from a long curving drive.
- Hemp Gill and Borrowdale, once a single large property, have their own separate approaches off Carriage Drive.
- The older properties in the area display decorative brickwork detail and distinctive design features (photo 15.4).
- Howey Lane dwellings have predominantly large plots and well-stocked secluded gardens, many fronted by low, sandstone walls. Some garden infill, property conversion and sensitive extension and alteration have taken place.
- Howey Rise, set against the wooded backdrop of Overton Hill (photo 15.5), comprises 1970s detached houses.
- Sandstone walls are prominent at the western end of Howey Lane and on Chester Road (photo 15.6; 15.11).
- Carriage Drive and Howey Lane are mostly without pavements and the latter has traffic calming measures (photo 15.6).
- Green spaces enhance the junction of Howey Lane and Chester Road and the entrance to Howey Rise (photo 15.7).
- The cast iron milepost on Chester Road near the junction with Howey Lane is a distinctive roadside feature (photo 15.12) (see similar milepost in TA06).
- Footpaths in this area link to the network of paths on Overton Hill and on to The Sandstone Trail (photo 15.8).

## ***Planning Guidance***

1. All new development should resist further plot division and infill to retain the individual distinctiveness and biodiversity of the area.  
(LPP: NE7; BE1)



2. All new development should resist demolition and replacement of Victorian and Edwardian properties to retain the diverse character of the area.  
(LPP: BE1; H4; H10)
3. All new development that includes subdivision of existing dwellings into self-contained residential units should ensure that the character of the original building is maintained or enhanced (photo 15.3).  
(LPP: BE1; H4; H7)
4. All new development that includes extensions and alterations to existing property should maintain the character of the original building, harmonise with the existing building and have respect for neighbouring properties.  
(LPP: BE1; H4; H8)
5. All new development should maintain and repair existing sandstone walls, copings and gate piers using traditional methods and materials (photo 15.9; 15.11).  
(LPP: BE1)
6. All new development should promote the area's green infrastructure and biodiversity and its links to open countryside (photo 15.9).  
(LPP: NE7; NE9; BE1)
7. All new development should protect and manage mature trees, hedges and green spaces wherever possible, e.g. on Howey Lane and Carriage Drive (photo 15.7; 15.10), to augment green infrastructure and the biodiversity corridors shown on **Map 2.2**.  
(LPP: NE7; NE8; NE9; BE1)
8. All new development should respect and enhance the setting of the cast iron milepost on Chester Road (photo 15.14).  
(LPP: BE1)
9. All new development should protect the view from Orchard Close to the War Memorial and from Carriage Drive to the marshes and the Mersey Estuary.  
(LPP: BE1)
10. Maintain safe bridleways and footpaths for shared use by horse riders, pedestrians and cyclists  
(LPP: T8; T9)

### ***Community Aspirations***

- Local people desire that the character of the area's footpaths be maintained and promoted as traffic free routes.  
(photo 15.13) (LP: T8)
- The community hopes that properties on the list of locally important buildings, drawn up by VRBC in 1977-79, and other properties – some described as 'unlisted buildings of townscape merit' and identified in the Frodsham (Overton, St Lawrence's) Conservation Area Appraisal – will be assessed and/or re-evaluated for inclusion on a revised 21st century Local List.



- The community wishes to see the 30mph speed restriction zone begin near the Chester Road / Tarvin Road junction to ensure greater safety for vehicles turning at the 5 road junctions in Netherton; and into Castle Park Main Entrance.
- Energy and water efficiency measures and features are encouraged. Where these are proposed in any new development, it is recommended that developers should pay regard to both the Code for Sustainable Homes and BREAM guidelines.



15.1 Mature garden, Carriage Drive.



15.2 Rock Cottage and Rock Farm (LB), Chester Road.



15.3 Erindale (LIB) 1911, off Carriage Drive, now 3 residential units.



15.4 Locally distinctive brickwork on a Carriage Drive property.



15.5 Howey Rise.



15.6 Sandstone walls and traffic calming at the western end of Howey Lane.





15.7 Green space at the entrance to Howey Rise.



15.8 Footpath 31 from Howey Lane to Overton



15.9 Howey Lane from Pinmill Brow.



15.10 Mature trees along the curving drive to Erindale properties.



15.11 Distinctive sandstone wall at the west end of Howey Lane.



15.12 Milepost, Chester Road.



15.13 Footpath 32 from Howey Lane to Chester Road.



15.14 Setting of the milepost, Chester Road.



Section 4 References

1. CCC (1996) Frodsham Parish Paths

2. VRBC (2003) Frodsham (Castle Park) Conservation Area Appraisal

3. VRBC (2005) Frodsham (Overton, Five Crosses) Conservation Area Appraisal

4. VRBC (2005) Frodsham (Overton,St. Lawrence's) Conservation Area Appraisal

5. VRBC (2006) Frodsham (Town) Conservation Area Appraisal

6. VRBC (2006) Adopted Vale Royal Borough Local Plan First Review Alteration

7. VRBC (2006) Adopted Vale Royal Borough Local Plan First Review Alteration  
Proposals Maps

8. EH Register of Parks and Gardens (2002) Castle Park PG3508

9. Nature Conservancy Council (1990) Frodsham Railway & Road Cuttings SSSI  
notification

10. Cheshire RIGS (2002) Frodsham CH 036 Geological Site Report

11. NWDA (2008) North West Green Infrastructure Guide version 1.1

Appendix 1 – Local Plan Policies applicable within the Frodsham Town Policy Boundary by  
Townscape Area

LPP no.	TA01	TA02	TA03	TA04	TA05	TA06	TA07	TA08	TA09	TA10	TA11	TA12	TA13	TA14	TA15
General Strategy															
GS3		*		*				*							
Natural Environment															
NE1		*				*									
NE2			*							*					
NE3														*	
NE4		*												*	
NE7		*	*			*		*	*	*	*	*	*	*	*
NE8		*				*		*	*	*	*		*		*
NE9			*					*		*				*	*
NE15			*												
Built Environment															
BE1	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
BE4	*		*	*	*					*			*		
BE5	*		*			*				*					
BE6	*		*			*				*					
BE7	*		*							*					
BE8	*		*	*		*		*		*				*	
BE9	*		*												
BE10	*		*	*				*		*				*	
BE11	*		*	*				*		*				*	
BE15														*	
BE16			*			*									
BE19	*		*	*				*		*				*	
BE21	*		*	*										*	
BE22	*		*	*				*		*				*	
BE23	*		*	*				*		*				*	
Housing															
H4			*		*	*	*	*	*	*	*	*	*	*	*
H5			*		*			*	*	*	*				
H6															
H7								*	*	*	*		*		*
H8			*	*	*	*	*	*	*	*	*	*	*	*	*
H10			*					*	*	*	*	*	*	*	*
H15					*										
Recreation and Tourism															
RT2					*									*	*
RT3				*									*	*	
RT4		*		*	*		*			*		*	*	*	
Transportation															
T5			*	*	*	*	*	*	*	*	*	*	*	*	*
T8			*	*	*	*	*	*	*	*	*	*	*	*	*
T9					*	*	*						*		*
T10					*										
T13				*											
Shopping and Town Centre Development															
STC1			*												
STC4			*												
STC5			*												
STC6			*												
STC8			*												
STC9			*												
STC10			*												
STC11			*												



## Appendix 2 Diary of Events Leading to Frodsham Town Design Statement

**25 Nov 2003** John Gittins made a presentation to Frodsham Forward Environmental Working Group who embraced the project.

**Jan 2004** Frodsham Town Council agreed to support the TDS project and budgeted £500.  
04 May 2004 Initial meeting to discuss the scope for the TDS. It was agreed to incorporate a LCA. TDS team formed. Later in May Frodsham Forward confirmed £2000 budget for project.

**14 June 2004** Further meeting to agree on requirements based on professional guidance.

**23 Sept 2004** Training day for TDS team

**16 Oct 2004** Public meeting in the large hall at the Community Centre. This was a key event where public consultation led to the agreement on precise townscape area boundaries and volunteer survey teams were set up.

**Nov 2004 - March 2005** TDS survey work conducted by mini-teams using the Team's purpose-designed checklist to standardise information collected. All townscape areas surveyed (except for the strip of land between the M56 and the built environment). Photographs were taken of virtually every street in Frodsham. A spreadsheet was drawn up giving the key characteristics of each street and a cross reference to the photographs. This detailed work provided a sound basis for the summary pages in the final PLA and TDS document.

**April 2005 - June 2006** LCA work began. Volunteers concluded a desktop study of parish landscape components, e.g. geology, landforms, soils, agricultural land quality. Provisional boundaries between distinctive LCTs and LCAs were added to a map of the parish. A purpose-designed checklist was used for landscape survey work which included recording land use, land cover and viewpoints etc.

**Nov 2004 - June 2005** Further regular meetings took place to check on progress and formulate the best way forward at each stage.

**3 July 2005** Team manned a gazebo at "The Festival in the Park" to liaise with the local community and explain what had been done and the purpose. Future work was also communicated and the opportunity was taken to hand out forms for residents to give the TDS team feedback on their efforts.

**Aug 2005 - Dec 2005** Consolidation work. All TDS areas finalised and feedback from the July event dealt with. Further team meetings took place.

**2006** LCA refined to give 7 rural Landscape Character Types incorporating 19 Landscape Character Areas. Much detailed work ensued including the construction of topographic profiles, further photographs, challenges, opportunities and guidelines. Starting with Salt Works Farm a draft layout for the TDS section of the final project document was designed. It was agreed that the Townscape would form a Landscape Character Type (no. 8). Further team meetings were held throughout year.

**Feb - Mar 2006** Articles prepared for Frodsham Life and Frodsham Post to inform the local community of progress. £2000 secured from CCC to assist with printing costs.

**Sep 2006 - Dec 2006** TDS and LCT drafts prepared. Work on other sections shared out (intro, history, purpose etc.).

**Nov 2006** Further £2000 plus 2 days graphic design time secured from VRBC for publication. A meeting was called with VRBC officers re the public consultation process and Sustainability Appraisal.

**Feb 2007** Full draft ready for statutory and public consultation and forwarded to VRBC.

**May - July 2007** Updating of Jan. draft document prior to submission.

**1 July 2007** Team manned a gazebo at "The Festival in the Park" to inform the local community and explain what had been done and the purpose. Names of residents interested in being involved in public consultation process were collected.

**15 Aug - 26 Sept 2007** Statutory and public consultation period

**12 Nov 2007** Advisory meeting at Wyvern House with VRBC officers, CLT, plus GONW and English Heritage personnel to discuss responses, followed by further meetings to plan the new way forward for the Frodsham TDS

**March 2008 - May 2009** Re-drafting of document in line with comments made by GONW and English Heritage, plus a re-survey of the town to incorporate changes made since June 2005.

**3 May 2008** - Frodsham Festival of Walks - 'Four Hills of Frodsham' walk led by 2 of the TDS Team to communicate information about the Parish landscape.

**26 March 2009** Advisory meeting at Wyvern House with VRBC officers, GONW and CLT, followed by further revision.

**26 April 2009** Frodsham Festival of Walks - 'Market Town and River Port' walk led by 2 of the TDS Team to celebrate Frodsham's octocentenary and communicate information about the town's heritage assets.

**May 2009** Updated draft document forwarded to Cheshire West and Chester to begin the process leading to statutory and public consultation.

**2 Sept - 14 Oct 2009** Statutory and public consultation period

**19 Nov** Advisory meeting at Backford Hall with VRBC officers, CLT, and an English Heritage representative to discuss responses, followed by final editing.

**31 Jan** Submission of Frodsham Town Design Statement Supplementary Planning Document.

**May 2010** Final checking of document.

**22 July 2010** The Town Design Statement is formally adopted as a Supplementary Planning Document by Cheshire West and Chester Council.



THE ORIGINAL TEAM

Facilitator: John Gittins (Cheshire Landscape Trust)

Frodsham Residents:

- Ethel Barker
- Norman Barker
- John Beech
- Tom Blundell
- Graham Bondi
- Ann Gardiner
- Kath Gee
- Len Johnson
- Brian Lloyd
- Christine Lloyd
- Juliet Swift
- Peter Swift
- Jenny Woodward
- Andrew Worrall

Resurvey and final document prepared by:

- Ann Gardiner
- Kath Gee

Editing of text and drafting of maps conducted by:

- Peter Dutton
- Kath Gee

Front Cover Photograph

Lynda Haney (Frodsham and District Photographic Society).

Support from various personnel at Vale Royal Borough Council and at Cheshire West and Chester Council, especially Phill Bamford, is gratefully acknowledged.

Appendix 3.1 - Listed Buildings in Frodsham TPB by Townscape Area

TA no./name	Listed Buildings	45
01 – Marsh Lane	Main St: Millbank House, 1 Main St.; 1 & 2 Millbank Cottage; Brook House, 38 Main St.	3
02 – Motorway Buffer Zone		
03 – Core Commercial Core	N side Main St: 47, 49 & 51; 53, 55 & 57 (Brookstone Cottages); Ashley House, 59; Old Hall Hotel, 81; 2 Tide-stones at OHH; N & W garden wall at OHH; 83 (Thatched Cottage); 85 & 89 (Thatched Cottages); 101 & 103 (Millmark & Coral); K6 Telephone kiosk to front of 101 & 103; The Bear’s Paw Hotel. S side Main St: 46 & 48; 52 (The Gables); Entrance to army premises; 84 (Oak-framed cottage); 90 (Les Harris Jewellers); 92; 108 & 110; Queen’s Head Hotel; Stable behind QHH; Lloyds TSB (Trustee Savings Bank); Golden Lion. Church St: Frodsham Railway Station; K4 Telephone Kiosk; 1 Church St. High St: 4 (Amore); 44 (The Old Cottage); Rock Villa, 67.	28
04 – Ashton Drive		
05 – Saltworks Farm		
06 – Frodsham Bridge	Frodsham Bridge; Frodsham Viaduct; Cottage and coach/cart shed 30m E of Manor Farmhouse; L-shaped stable/haybarn /shippon 50m SE of MF; Shippon 10m S of MF.	5
07 – The Lakes		
08 – Doric Avenue		
09 – Five Crosses		
10 – Overton	Church Farm (Church House Farmhouse); Church of St Laurence Grade I; Sundial nr S porch; Wright tomb nr W entrance; Ring o’ Bells Inn; 56 Hillside Rd formerly Flower Cottage; Overton House, School Lane; The Old Vicarage, Vicarage Lane Grade II*;	8
11 – Fluin Lane		
12 – Community Centre		
13 – Kingsway		
14 – Castle Park		
15 – Howey Lane	Rock Cottage and Rock Farm, Chester Rd;	1

Sources

- Listed Buildings in Vale Royal November 2000
- Listed Building descriptions for Frodsham Civil Parish Pers Comm Vale Royal Conservation and Design Manager 2007



Appendix 3.2 - Locally Important Buildings in Frodsham TPB by Townscape Area

TA no./name	Locally Important (Locally Listed) Buildings	78
01 – Marsh Lane 10	N side Main St: Millbank Cottage; Cheshire Cheese, 29; 31 & 33 (3-storey semis); Millstone House, 43. S side Main St: Bourne Methodist Chapel; 14 (mid terrace); 20 (detached house); 26 & 28 (terraced). Marsh Lane: 1-13 Ashley Gardens; 1-14 Moreton Terrace.	
02 – Motorway Buffer Zone		
03 – Commercial Core 39	N side Main St: 75 & 77; 91-99 (Residential terrace between Thatched Cottages & Millmark); K6 kiosk adjacent, & west of, listed K6 outside 101 (1); Nat West, 117; 119 (Lingerie Box); 119A & 121 (Haircraft & Cottage Teashop); 123 (former Buy & Sell). S side Main St: 2 & 4 Fountain Ln; 42; Pavement adjacent to wall of 52, The Gables (1); Drill Hall; 68 (Eve); 70 & 72 (Frodsham Frames); 74 Conservative Club; 78A Barn (behind Halton Haven C Shop); [84A now part of 84 – LB]; 88; Outbuildings, Queen's Head, 94;100. Church St: Warehouse behind 6 (Hough & Co); 8 (Suzanne's); Cholmondeley Arms, 12; 3 (Halifax). N side High St: 9 (cottage W of Turner's); Red Lion, 17; Tandoori 25 (George Inn); 1 Ship St (Foot Clinic); Former Court House, Ship St (Guiding HQ); Former Police HQ (part of Wedding Belles); 27 (brick terraced house); 37 (brick end of terrace); The Cottage, Albert Row (The Joiners Cottage). S side High St: 28, 30 & 32 (Fishermen's Cottages); 40; Rock House, 48; 50; Rock Cottage, 52; The Knoll; Trinity Methodist Church ruins and Spire; Trinity House; 78.	
04 – Ashton Drive 4	Ship St: 26; 28 & 30 (26 & 28 now The Cottage); Rose Cottage, 59; Hawthorne Rd: Pear Tree Cottage/Farm (derelict)	
05 – Saltworks Farm		
06 – Frodsham Bridge 4	Bridge Lane: 1 & 1A; Union Church (apartments); East Bank; East Bank Cottage.	
07 –The Lakes		
08 – Doric Avenue 3	Vicarage Rd: "High Lea"; West View; Overton Hall.	
09 – Five Crosses 3	Eversley House; Top Rd: 11 & 13; 19-25.	
10 – Overton 7	C of E Primary School, School Lane; Scout Hall, Manley Road (Five Crosses Infants School); Outbuilding adjacent to Ring o' Bells, Bellemonte Rd (2); Aubrey House, Bellemonte Rd; Pinmill Brow House, 26 Howey Lane (with picket fence); Roebuck House, Church Rd; Rose Ville, 65 Hillside Rd.	
11 – Fluin Lane 1	Brockton Mews, Church Rd.	
12 – Commun-ity Centre		
13 – Kingsway		
14 – Castle Park (3) 5	5 "Key Buildings": Castle Park House; Footman's Cottage; Castle Park Arts Centre; Park Court; Conservatory.	
15 – Howey Lane 1	Erindale, Carriage Drive.	

Sources

Adopted Vale Royal Borough Local Plan First Review Alteration June 2006/ Appendix 7

Frodsham (Town) Conservation Area Appraisal Adopted September 2006 (1)

Frodsham (Overton, St Lawrence's) Conservation Area Appraisal August 2005 (2)

Frodsham (Castle Park Locality) Conservation Area Appraisal September 2003 (3)



Appendix 3.3 - Unlisted Buildings of Townscape Merit in 2 of 4 Conservation Areas by Townscape Area

TA no./name	Unlisted Buildings of Townscape Merit	Conservation Area
01 – Marsh Lane	22	Frodsham (Town)
02 – Motorway Buffer Zone		Frodsham (Town)
03 – Commercial Core		Frodsham (Town)
04 – Ashton Drive		Frodsham (Town)
05 – Saltworks Farm		
06 – Frodsham Bridge		
07 – The Lakes		
08 – Doric Avenue 7	Bradley Ln: Former Sunday School. Kingsley Rd: 1 & 2 (Little Hurcot and Old Post Office); Oakdale. Oakdale Avenue: 1 (Tynedale); 3 (High Nest); 5 & 7 (The Beeches and The Elms); 9 (The Limes)	Overton, Five Crosses
09 – Five Crosses 2	Kingsley Rd: Frodsham Methodist Church; Ingledene.	Overton, Five Crosses
10 – Overton 9	Vicarage Lane: Overton Memorial Hall; White Cottage; Oaklands; Lynden.  Howey Ln: 24 (rendered house on rock); 25 (house on S side of road); Pin Mill Cottage, Pin Mill Brow (seen from Hillfield). Bulls Head, Bellemonte Rd. Inglenook, 1A Hillside Rd.	Overton, Five Crosses  Overton, St Lawrence's
11 – Fluin Lane 2	Fluin Ln: 93 & 95; Lower Brockton.	Overton, St Lawrence's
12 – Community Centre		
13 – Kingsway		Frodsham (Castle Park Locality)
14 – Castle Park		Frodsham (Castle Park Locality)
15 – Howey Lane 2	Howey Lane: 33 & 35 (corner of Bottom Walk); 43.	Overton, St Lawrence's

Sources

Frodsham (Overton, St Lawrence's) Conservation Area Appraisal Map August 2005  
Frodsham (Overton, Five Crosses) Conservation Area Appraisal Map August 2005

Glossary

- Alteration:** Work intended to change the function or appearance of a place.
- Biodiversity Corridor:** Continuous strips of land or 'stepping stones' that are patches of suitable habitat used to link nature sites together and encourage the movement of plant and animal species so as to prevent species isolation.
- British Research Establishment Environmental Assessment Method (BREEAM):** BREEAM is the most widely used environmental assessment method for buildings. Using a scoring system, it sets the standard for best practice in sustainable design and measuring the environmental performance of new non-residential building.
- Buildings at Risk:** A register, published annually which brings together information on all Grade I and II\* listed buildings and scheduled monuments known to English Heritage to be "at risk" through neglect and decay or vulnerable to becoming so.
- Cheshire Historic Environment Record (CHER):** A database recording all known archaeological sites and historic landscapes within Cheshire. It includes sites dating from prehistory through to the military remains of the more recent past.
- Code for Sustainable Homes:** Voluntary code aiming to improve the environmental sustainability of houses and to provide homeowners better information about the running costs of their homes. The Code sets environmental standards that can be applied to all homes. There are six levels to the code to communicate the overall sustainability performance of a new home, with minimum energy efficiency, carbon emissions and water efficiency standards set at each level.
- Conservation:** The process of managing change to a significant place in its setting in ways that will best sustain its heritage values, while recognising opportunities to reveal or reinforce those values for present and future generations
- Conservation Area:** Areas of special architectural or historic interest, the character or appearance of which it is desirable to preserve or enhance. Such areas are designated by Local Planning Authorities under the Town and Country Planning Acts.
- Context:** Any relationship between a place and other places, relevant to the values of that place.
- Core Strategy:** The key document within the Council's Local Development Framework, setting out the core planning policies for the Cheshire West and Chester authority area.
- Designation:** The recognition of particular heritage value(s) of a significant place by giving it formal status under law or policy intended to sustain those values.
- DCLG:** Department for Communities and Local Government (formerly ODPM – Office of the Deputy Prime Minister).
- Density (of dwellings):** Relates to the number of dwellings per hectare on a site

**Development:** Defined in the Town and Country Planning Act as “the carrying out of building, engineering, mining or other operations in, on, over or under land, or the making of any material change in the use of any buildings or other land.”

**Fabric:** The material substance of which places are formed, including geology, archaeological deposits, structures and buildings and flora.

**Green Infrastructure:** The network of natural environmental components and green and blue spaces that lie within and between cities, towns and villages and which provide multiple social, economic and environmental benefits.

**Greenfield:** Land on which no development has previously taken place unless the previous development was for agriculture or forestry purpose or, the remains any structure or activity have blended into the landscape

**Green Belt:** Area of land, largely rural in character, which is adjacent to the main urban areas and which is protected from development by permanent and severe restrictions on building. The emphasis is on restricting the sprawl of urban centres, preventing the coalescence of neighbouring towns and preserving the individual character of settlements, although Green Belts may also provide suitable locations for recreational development and act as a buffer between the most rural countryside and the pressure of growing towns.

**Heritage:** All inherited resources which people value for reasons beyond mere utility.

**Historic Environment:** All aspects of the environment resulting from interaction between people and places through time, including all surviving physical remains of past human activity, whether visible or buried, and deliberately planted or managed flora.

**Integrity:** Wholeness

**Landscape Character:** The distinct, recognisable and consistent pattern of elements in the landscape that makes one landscape different from another, rather than better or worse.

**Listed Building:** A building included in a list compiled by the Secretary of State for National Heritage as being of special architectural or historic interest.

**Local Development Framework (LDF):** A portfolio of local development documents which include the local development scheme (LDS), development plan documents (DPD’s), supplementary planning documents (SPD’s), the Statement of Community Involvement (SCI) and the Annual Monitoring Report (AMR). Together with Regional Spatial Strategy (RSS) these documents provide the planning framework for the authority area.

**Local Development Scheme (LDS):** A public document setting out the Council’s programme for the production of its Local Development Documents.

**Local Plan:** A plan proposed or adopted by a Local Planning Authority. Local Plans produced by the three former district authorities which now form part of Cheshire West and Chester provide the current statutory planning framework within the authority area.

**Maintenance:** Routine work regularly necessary to keep the fabric of a place on good order.

**Material:** Relevant to and having a substantial effect on, demanding consideration.

**Monitoring:** Continuous survey aimed at discovering and measuring significant deviations from a plan of its underlying assumptions.

**Nature Conservation:** The planning and management of wildlife and habitats so as to secure their wide use and continuity of supply while maintaining and enhancing their quality, value and diversity.

**Open Countryside:** The rural area outside the settlement policy boundaries for towns and villages

**Open Space:**

- |                 |   |
|-----------------|---|
| <b>Formal</b>   | Comprises a variety of uses including playing fields, bowling greens, golf courses, cricket pitches, tennis courts, equipped children’s play areas, parks etc   |
| <b>Informal</b> | Areas which fulfil a recreational/amenity function but which do not have a specific use, hence the term “informal” open space. They include areas of open space within housing estates used for informal play, kick about areas, dog walking areas etc. |

**Place:** Any part of the historic environment, of any scale, that has a distinctive identity perceived by people.

**Preserve:** To keep safe from harm.

**Redevelopment:** Development that takes place after the demolition of existing buildings.

**Regionally Important Geological/Geomorphological Sites (RIGS):** Sites of geological or geomorphological importance other than SSSIs which are considered worthy of protection. They are selected in Cheshire by the Cheshire RIGGS group.

**Regional Spatial Strategy (RSS):** A document prepared by regional planning bodies which sets a spatial development framework for the region.

**Renewable Energy:** Energy flows that occur naturally and repeatedly in the environment, i.e. from the sun, wind and water as well as heat generated from within the earth itself and energy derived from plant material and from combustible or digestible industrial, agricultural and domestic waste materials.

**Repair:** Work beyond the scope of maintenance, to remedy defects caused by decay, damage or use, including minor adaptation to achieve a sustainable outcome, but not involving restoration or alteration.



**Restoration:** To return a place to a known earlier state, on the basis of compelling evidence, without conjecture.

**Retail Use Classes:**

- A1** Shops, post offices, retail, warehouses, ticket and travel agencies, sandwich bars, hairdressers, funeral directors, showrooms, domestic/personal hire shops, dry cleaners/washing service.
- A2** Banks, building societies, estate and employment agencies, professional and financial services, betting offices, other services appropriate in a shopping area (principally serving visiting members of the public.
- A3** Restaurants & Cafes
- A4** Drinking Establishments, public houses, wine bars
- A5** Hot Food Take-Away

**Setting:** The surroundings in which a place is experienced, its local context, embracing present and past relationships to the adjacent landscape.

**SSSI - Site of Special Scientific Interest:** An area of land identified and designated by English Nature as being of special national interest by reason of any of its flora, fauna or geological or physiological features.

**SPD – Supplementary Planning Document:** Guidance notes issued by the Council which give advice on particular aspects of development and provide further guidance on policies contained within the Local Plan. Formerly known as SPGs.

**Sustainable Development:** Defined as “Development that meets the needs of the present without compromising the ability of future generations to meet their needs”.

**Sustainable Urban Drainage Systems (SUDS):** Surface water drainage methods developed in line with the ideals of sustainable development, taking account of quantity, quality and amenity issues.

**Traffic Management:** The promotion of the more efficient movement of traffic and appropriate use of street systems by rearranging the flows, controlling the intersections and regulating the times and places for parking by means of traffic orders. Traffic management can give priority to different forms of transport, such as buses or to pedestrians.

**Value:** An aspect of worth or importance, here attached by people to qualities of places.

**Wildlife Corridor:** A tract of land following a common feature (e.g. river, valley, road) designated for its local ecological value in the former Vale Royal authority area in order to conserve the natural habitats and the wildlife they support.